

93 00670

INSTITUTE OF GOVERNMENTAL
STUDIES LIBRARY

APR 29 1993

UNIVERSITY OF CALIFORNIA

NORTHGATE

Vallejo, California

NORTHGATE

SPECIFIC AREA PLAN

Vallejo, California

Adopted by the Vallejo City Council on October 25, 1988

Amended by the Vallejo City Council on September 26, 1989

Compiled December 1, 1992

TABLE OF CONTENTS

SECTION 1. INTRODUCTION.....	1.
1.1 Purpose of Northgate Specific Area Plan.....	1.
1.2 Organization of Northgate Specific Area Plan.....	1.
1.3 Geographical Area of Application.....	1.
1.4 Goals of the Northgate Specific Area Plan.....	1.
1.5 Summary of the Northgate Planning Proposal.....	3.
1.5.1 Office Park.....	3.
1.5.2 Business Park.....	4.
1.5.3 Commercial Center.....	4.
1.5.4 Low Density Residential Neighborhoods.....	4.
1.5.5 Medium Density Residential Neighborhoods.....	5.
1.5.6 Public Facilities.....	5.
1.5.7 Open Space.....	5.
1.6 Plan Area Setting.....	5.
1.6.1 Location, History and Current Uses.....	5.
1.6.2 Regional Context.....	6.
1.6.3 Topography, Vegetation and Wildlife.....	9.
1.6.4 Geology.....	10.
1.6.5 Soils.....	10.
1.6.6 Seismicity.....	11.
1.6.7 Drainage.....	11.
1.6.8 Circulation.....	14.
1.7 Existing Infrastructure.....	15.
1.7.1 Sanitary Sewer.....	15.
1.7.2 Natural Gas and Electric.....	15.
1.7.3 Telephone.....	15.
1.7.4 Water.....	15.
1.7.5 Solid Waste.....	15.
SECTION 2. NORTHGATE DEVELOPMENT PLAN.....	16.
2.1 Land Use and Development Plan.....	16.
2.2 Relationship to the Vallejo General Plan.....	19.
2.3 Consistency with the Vallejo General Plan's Goals and Policies.....	19.
SECTION 3. THE OFFICE PARK LAND USE ELEMENT.....	20.
SECTION 4. THE BUSINESS PARK LAND USE ELEMENT.....	21.

SECTION 5. THE COMMERCIAL LAND USE ELEMENT.....	22.
SECTION 6. RESIDENTIAL NEIGHBORHOOD ELEMENT.....	24.
6.1 Low Density Residential Neighborhoods.....	25.
6.2 Medium Density Residential Neighborhood.....	26.
SECTION 7. URBAN RESERVE ELEMENT.....	27.
SECTION 8. PUBLIC FACILITIES AND SERVICES ELEMENT.....	27.
8.1 Concept.....	27.
8.2 Fire Protection.....	27.
8.3 Police.....	27.
8.4 Elementary School.....	28.
8.5 High School.....	28.
8.6 Parks.....	28.
8.7 Park and Ride Facility.....	28.
SECTION 9. CIRCULATION ELEMENT.....	28.
9.1 Concept.....	28.
9.2 Existing Streets.....	29.
9.3 Planned Streets.....	29.
9.4 Pedestrian Paths and Trails.....	32.
SECTION 10. OPEN SPACE ELEMENT.....	33.
10.1 Concept.....	33.
10.2 Characteristics.....	33.
10.3 Open Space Disposition.....	33.
SECTION 11. PUBLIC INFRASTRUCTURE ELEMENT.....	34.
11.1 Water.....	34.
11.2 Sewer.....	34.
11.3 Storm Drainage.....	34.
11.4 Utilities.....	35.
11.5 Streets.....	35.
11.6 Street Lighting.....	35.
11.7 Grading.....	35.
SECTION 12. IMPLEMENTATION.....	38.
12.1 Basic Goals and Policies.....	38.
12.2 Development Policies and Standards.....	38.
12.3 Phasing Program.....	38.
12.4 Enforcement of Standards.....	39.

12.4.1 Conditions, Covenants, and Restrictions (CC&Rs).....	39.
12.4.2 Zoning.....	40.
12.5 Funding of Capital Improvements and Public Services.....	40.
12.5.1 Financing.....	40.
12.5.2 Construction Responsibility and Maintenance Responsibility.....	40.
SECTION 13. IMPLEMENTATION PROCESS.....	43.

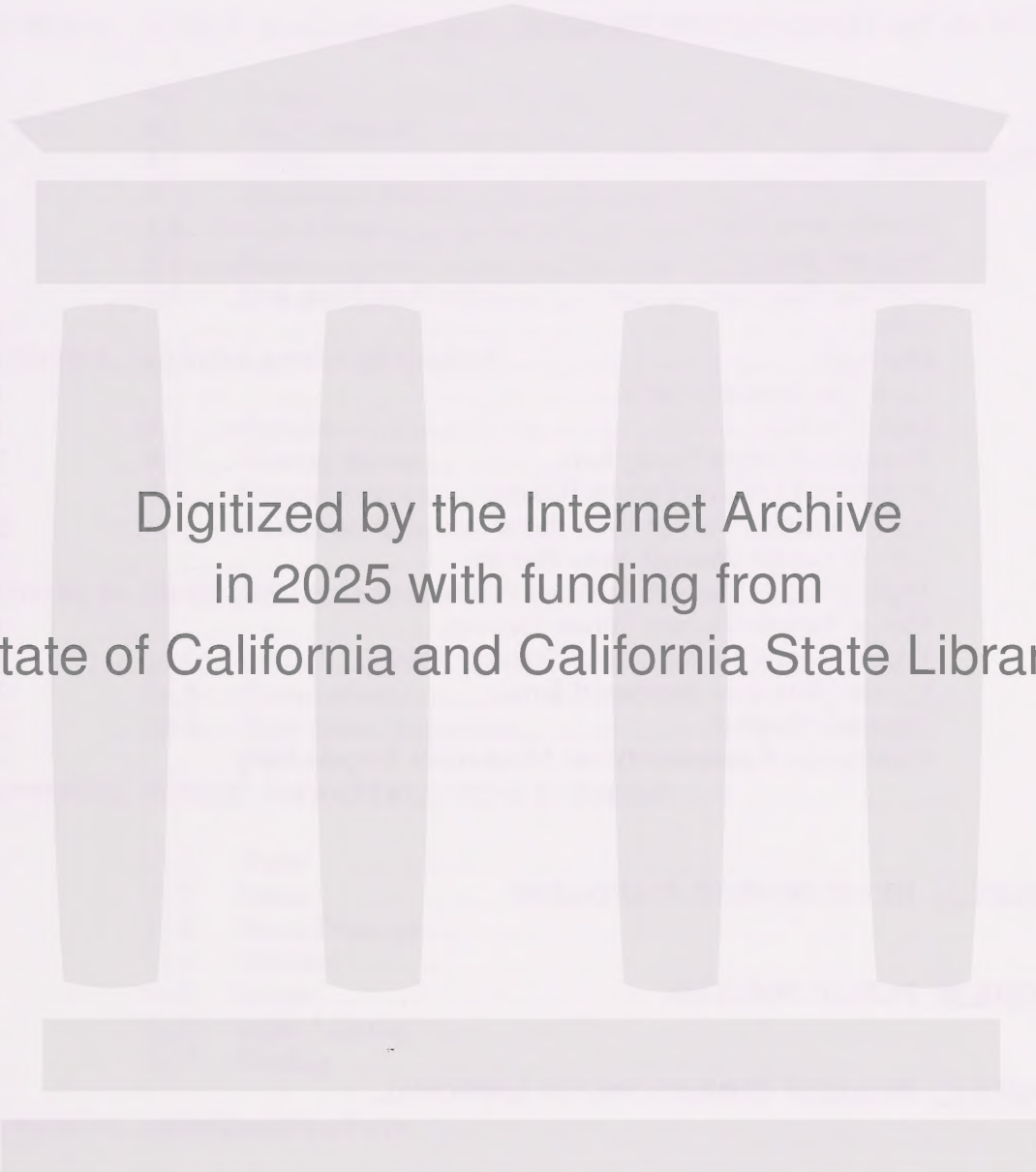
LIST OF FIGURES

Specific Area Plan.....	2.
Regional Map.....	7.
Vicinity Map.....	8.
Soils.....	12.
Drainage.....	13.
Land Use Summary Table.....	17.
Land Use Map.....	18.
Prototypical Single Family Lot.....	25.
Prototypical Medium Density Housing.....	26.
Typical Section Through Plaza Drive.....	30.
Typical Section Through Ascot Parkway.....	31.
Typical Section Through Redwood Parkway.....	31.
Typical Section Through Turner Parkway.....	31.
Typical Section for Residential Collector Street.....	32.
Typical Section for Residential Street.....	32.
Schematic Grading.....	37.
Construction Responsibility and Maintenance Responsibility.....	41.

APPENDIX A: DEVELOPMENT STANDARDS

APPENDIX B: PUBLIC POLICIES

APPENDIX C: PROJECT CONDITIONS OF APPROVAL



Digitized by the Internet Archive
in 2025 with funding from
State of California and California State Library

<https://archive.org/details/C124909401>

SECTION 1. INTRODUCTION

1.1 PURPOSE OF THE NORTHGATE SPECIFIC AREA PLAN

The Northgate Specific Area Plan (SAP) proposes a comprehensive plan for the development of an Office Park, a Business Park, residential neighborhoods, Commercial Center, and attendant schools and open space. Upon adoption, this SAP will represent the City of Vallejo's development policy for the area.

This Specific Area Plan will serve as the Master Plan for the Northgate Planned Development District. It is the commitment by Vallejo Development Company or its successor-in-interest to merge the requirements for economically sound development with the unique physical characteristics of the site, and to produce a project that is consistent with City goals and compatible with the existing urban environment.

1.2 ORGANIZATION OF THE NORTHGATE SPECIFIC AREA PLAN

This Specific Area Plan is organized into thirteen sections which cover every aspect of the project and its processing by the City and other public agencies. Each of these agencies will further explain the proposed development and will detail specific aspects and concerns for each of the land use elements as well as establish design and development standards. Additionally, improvements to the public utility infrastructure and public facilities and services will be addressed. Lastly, a review of the implementation process and public policies will be presented.

(Note: This SAP was adopted in October 1988 and amended in September 1989, and both times the City Council imposed conditions on the project. These conditions have been incorporated into the SAP where appropriate. However, they are included in their entirety with this SAP as Appendix C.)

1.3 GEOGRAPHICAL AREA OF APPLICATION

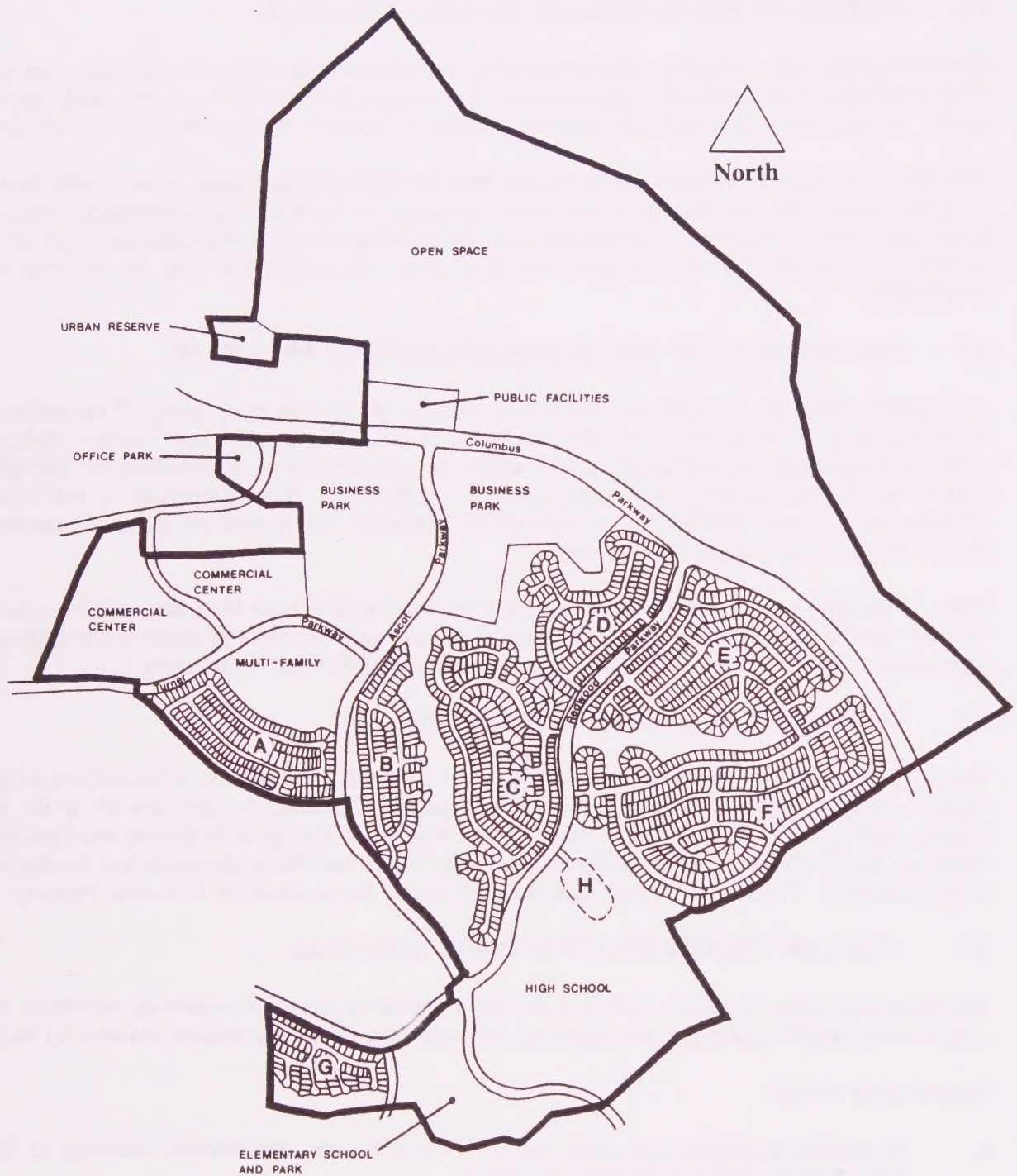
The 1,165-acre site, which includes most of the former Hunter Ranch property, is located in the City of Vallejo's Northeast Quadrant. The project area is generally bordered by Interstate 80 to the west, Sulphur Springs Mountain along the north and east, Blue Rock Springs Golf Course and Park to the southeast, Blue Rock Springs to the south, and the Hunter Ranch and Woodridge residential developments to the southwest. The area is bisected from the northwest to the southeast by Columbus Parkway.

1.4 GOALS OF THE NORTHGATE SPECIFIC AREA PLAN

The major goal of the Northgate SAP is to develop a unified business and residential community in an economically feasible manner, while respecting and preserving the unique physical character of the site.

General goals include:

- a. To establish an employment center with a variety of business opportunities, including an Office Park, Business Park and Commercial Center.



Northgate Specific Area Plan

Vallejo, California

- b. To provide a variety of housing opportunities including low density single family and medium density multi-family residential.
- c. To retain portions of the site to act as natural buffers between land uses.
- d. To provide a comprehensive circulation network, including vehicular access and bicycle/pedestrian facilities.
- e. To preserve Sulphur Springs Mountain as a visual amenity and wildlife habitat.
- f. To design projects so that within the project area there will be no net loss of wetland habitat as a result of the project.
- g. To abide by the Blue Rock Springs Corridor Master Plan, which include protection and provisions for public access to Blue Rock Springs Creek.
- h. To relocate, stabilize, and enhance Rindler Creek as an open space corridor for passive recreation, wildlife and flood control purposes.
- i. To provide and maintain public services and facilities in a manner that will not burden the existing level of service of public services and facilities within the city.
- j. To design projects so that views of the project area shall be enhanced and protected through the use of natural land forms, adequate landscaping and screening; and to promote quality architectural design for all structures.
- k. To provide adequate circulation into the project so as not to impact existing housing.

1.5 SUMMARY OF THE NORTHGATE PLANNING PROPOSAL

The SAP is the result of a careful interweaving of site characteristics and economic study conclusions. In accordance with the City of Vallejo's General Plan goals, the Northgate Project will "complement the city as a whole" and be a self-supporting community by providing a variety of residential uses, an employment center, schools and open space for passive recreational opportunities.

The geographic character of each of the site's three topographically distinct areas (grassland, rolling hills and a steep, unstable hillside) guided this Specific Area Plan.

1.5.1 The Office Park

The 4.1-acre Office Park is located on Admiral Callaghan Lane above Columbus Parkway. This highly visible site is near the Commercial Center and Business Park. The proposed Office Park will contain a number of separate office buildings, each with their own required parking support and landscaping. All buildings will be designed in accordance with the architectural guidelines and will be coordinated and integrated with each other.

The intended users of this Office Park will be "clean" users, such as administrative and professional services, communication services, medical services and day care. Heavy industrial users will not be allowed.

1.5.2 Business Park

The 105.9-acre Business Park, although more diverse in its users, is designed to reflect the same high quality development image established by the Office Park. Fronting on Columbus Parkway and split by the extension of Ascot Parkway, these parcels are easily accessible from the freeway which reduces the traffic impacts and conflicts with both the residential and office use on the Plan and existing uses in the area.

In order to ensure compatibility and consistency within the Business Park, Conditions, Covenants and Restrictions (CC&R's) will specify in detail what land uses are to be permitted. A list of permitted uses is included in Section 4. Building setbacks, landscape and site design guidelines are also provided by this SAP to set the tone of the development.

1.5.3 Commercial Center

The 62.3-acre Commercial Center has been placed adjacent to the existing Vallejo Corners Shopping Center to the west. It is north of Turner Parkway, and on either side of Plaza Drive. The proposed mixture of retail and commercial uses will complement the proposed Office Park and will help to increase available retail to the community.

As with the Office and Business Parks, CC&R's and Design Guidelines will be established to control the signage, landscape and screening that will be required.

(Note: The Commercial Center is now known as Gateway Plaza.)

1.5.4 Low Density Residential Neighborhoods

Predominantly in the central to eastern portion of the Plan the Low Density Residential neighborhoods will be created. The SAP shows eight neighborhoods that will not exceed 1,524 total units on a total of 312.5 acres. These neighborhoods are served primarily by Ascot and Redwood Parkways and border and incorporate the existing Blue Rock Springs Golf Course and areas of open space. These parcels are the core of the SAP.

These neighborhoods are intended to provide a broad range of housing types, costs and densities. As in any development of this size, what the particular mix of these different types of housing will be is determined by the current marketing conditions. Flexibility is intended in this SAP that will allow for the adjustment of the housing mix within the framework established by the SAP.

It is also this Specific Area Plan's intention to create residential neighborhoods that are separated by open space and major streets. The individual neighborhoods will all have well designed entries from the street and will be buffered from the street by soundwalls and landscaping.

The design of these neighborhoods will use the property's topography and elevation to create a mixture of lots that have views and privacy. These low density neighborhoods will also be placed

so that they buffer the existing single family areas to the south from the proposed commercial and office uses.

These neighborhoods have also been located to minimize the impact on the existing residential neighborhoods and existing circulation.

1.5.5 Medium Density Residential Neighborhood

A 26.3-acre site has been designated as multi-family, medium density residential, with a maximum density of 16.4 units per acre for a maximum of 432 units. This site is located at the southwestern corner of the intersection of Turner and Ascot Parkways. This site separates the single family lots from the business and commercial areas.

The site plan for this residential type will be characterized by a heavily landscaped perimeter, parking areas hidden from the street and private recreation facilities for the residents.

1.5.6 Public Facilities

Public facilities in the Plan include an elementary school/park site and high school/park site along the Blue Rock Springs Linear Park Corridor, and a fire station, a park and ride facility, power substation, and City water pumping station at the intersection of Columbus Parkway and Ascot Parkway.

1.5.7 Open Space

An important design element of the Northgate Project is the retention of open hillsides to the north of Columbus Parkway and below the Sulphur Springs Mountain ridgeline. Used both as a buffer between urban land uses and as a visual amenity, 421.8 acres have been left undeveloped.

The open space areas south of Columbus Parkway will be designated into the fabric of the land plan and will be used to separate and buffer the various land uses from each other. Controlled access will be limited throughout the open space to minimize the fire danger and erosion, and to insure privacy for the residents. Controlled access will be provided for maintenance and fire protection.

1.6 PLAN AREA SETTING

1.6.1 Location, History and Current Uses

The 1,165-acre site, which includes most of the former Hunter Ranch property, is located in Vallejo's Northeast Quadrant. The project area is generally bordered by Interstate 80 to the west, Sulphur Springs Mountain along the north and east, Blue Rock Springs Golf Course and Park to the southeast, Blue Rock Springs Creek to the south, and the Hunter Ranch and Woodridge residential developments to the southwest. The area is bisected from the northwest to the southeast by Columbus Parkway.

In the late 1850's the project area became the property of James and Andrew Hunter, and it was used primarily for cattle grazing. During the 1860's and 1870's, three houses were constructed in which the Hunters lived. The one house that remains from this era is adjacent to the project area on the NDS Investment Corporation property. The property has been used for access to the Vallejo White Sulphur Springs (a fashionable resort of the late nineteenth century, now Blue Rock Springs Park) and to St. John's mercury mine to the north of the property.

Today the site is still primarily used for cattle grazing. It is regularly seeded and mowed for cattle feed as well. The northern portions of the site are relatively unused because of steep slopes and unstable soils. There are two PG&E powerline easements running north and south on the westerly portion of the site. Two City of Vallejo water tanks are also located on the site. One tank is on a highly visible hilltop just east of the Hunter Ranch residential development. The other water tank is located farther to the east near Columbus Parkway. Columbus Parkway is the freeway access route for the trucks traveling to and from the Syar Industries quarry located southeast of the project area.

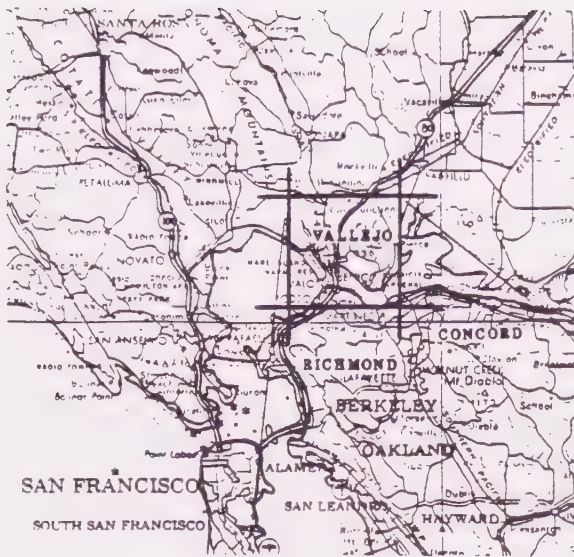
In 1982, a Master Plan for the Lundeberg Properties (Northeast Quadrant) was prepared. The proposed project included 300+ acres of office campus and business park with a maximum floor area of 8 million square feet. The major residential area included 236+ acres and a maximum of 2,320 units. This new SAP proposes a less intense and more economically feasible commercial, office and business area totalling 172.3 acres with a maximum floor area of approximately 1.9 million square feet. The housing will not exceed 1,956 units (including 1,524 single family units and 432 medium density units) over 338.8 acres. This SAP has been prepared in response to the Northgate Final Subsequent Environmental Impact Report, and it proposes a mitigated project.

1.6.2 Regional Context

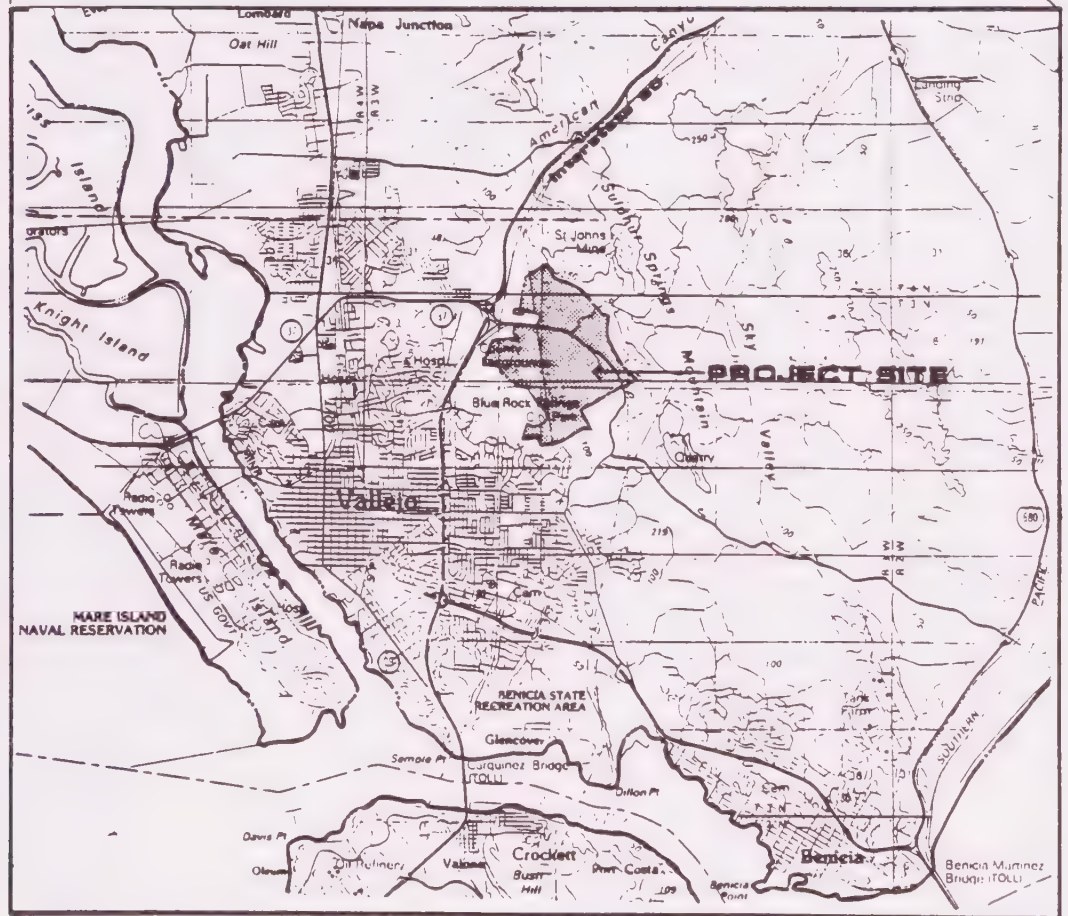
In terms of regional trends, Vallejo is identified by the Association of Bay Area Governments as one of the population growth centers of the Bay Area. This continued growth is expected to create a demand for major employment centers and more housing. Within the Vallejo, there is a desire to promote middle and upper income housing, as established in the "Housing Element" which was incorporated into the City's General Plan. The Northeast Quadrant is one of five areas established by the City in the mid-1970's for the purpose of promoting comprehensive planning of the city's remaining large tracts of land. This project will meet many of the City's goals for establishing an employment center and quality residential neighborhoods.

Backdropped by scenic Sulphur Springs Mountain, the large undeveloped property has excellent freeway access at the Columbus Parkway intersection. A variety of commercial, residential and recreational land uses presently surround the site. Directly west of the site and Interstate 80 is Marine World/Africa USA, a reflection of Vallejo's growing regional attraction. These immediate amenities make this site an ideal location for office, business, commercial and residential land uses.





VICINITY MAP



1.6.3 Topography, Vegetation and Wildlife

The varied topography provides excellent scenic opportunities. The site is framed to the north and east by the predominant ridge of Sulphur Springs Mountain and to the southeast by Blue Rock Springs Recreational Corridor and park and municipal golf course. Directly south and west of the site are newly developed single family housing subdivision. Looking west, there are distant views of the Bay.

The terrain is diverse with gentle hills, steep slopes, rock outcroppings and a predominant ridgeline. These characteristics allow the site to be divided into three topographic units which have influenced the proposed development on the site.

The first unit is the gently sloping grassland, near Admiral Callaghan Lane and the freeway. Because of the relatively gentle terrain and close proximity to the freeway, this area is better suited for uses requiring large lots with good freeway access, such as business and office.

The second unit is the interior portion of the site, comprised of smooth undulating hills. This area has been designated residential because the smaller lot sizes allow terracing up the hills and the varied elevations provide good views. The interior portion of the site is well removed from the freeway oriented uses and is bordered by various recreational uses (Blue Rock Springs Park and Golf Course, Blue Rock Springs Corridor and nearby ridgeland), making it even more appropriate for residential uses.

The third unit is the steep is the steep hillsides, rock outcroppings and windswept trees which dominate the north portion of the site and is an important visual asset to the region. The unstable soils in this area limit the development possibilities. As a result, this portion of the site will be left open as a visual and natural resource. Because the site has been heavily grazed, consideration may be given to the reestablishment of native species.

The existing vegetation of the site is predominantly grassland except for the eucalyptus groves adjacent to Blue Rock Springs Park and Golf Course and the few windswept trees on the ridgeline. The green irrigated landscape and tall trees of the golf course and park are a distinct contrast to the surrounding grassy hillsides, especially during the dry summer and autumn months.

The site's varied topography provides a diverse wildlife habitat including grassland, rocky hillsides, and intermittent wetlands along the creeks. A variety of birds nest and feed throughout the grassland, hillside and riparian areas, along with many other animal species such as lizards, snakes and ground squirrels. Of special concern is the presence of raptors (particularly the golden eagle) in the area. These birds rely heavily on the open grasslands and rocky hillsides for their winter foraging. As part of the Northgate proposal, over 400 acres of Sulphur Springs Mountain's grassy hillside will be retained in its present condition as a reserve for wildlife habitat.

The rolling hills provide both the greatest site amenity and the strongest site constraint. Existing features of the site (geology, soils, slopes and drainage) have been considered and incorporated into land use and site design in ways best suited to address environmental, economic and public safety issues.

1.6.4 Geology

To the north and east of Columbus Parkway, the project area is characterized by slopes averaging 20%, and extending from a maximum elevation of 800 feet to a minimum at road level of 150 feet. South of Columbus Parkway, the rolling hills have a maximum elevation of 480 feet in the easterly portion of the project area to 300 feet in the westerly portion. Slopes range from 10% to 25%.

The project area is underlain primarily by sedimentary rocks consisting of shale and sandstone. These rocks are stratified and individual beds vary from a few inches to several feet in thickness.

The stratification is oriented generally to the north-northwest and is inclined at angles from about 30 to 80 degrees to the south-southwest.

Rocks from the Franciscan formation constitute the prominent outcrop along the ridgetops northeast of Columbus Parkway. They consist of erosion-resistant masses of chert, sandstone and green stone and also include masses or lenses of serpentine. Pleistocene alluvium covers the valley bottoms and consists of heterogenous mixtures of gravel, sand, silt and clay. A geotechnical study has been completed by Harding Lawson Associates, giving a more detailed report of the site conditions. This study is incorporated into this SAP by reference.

1.6.5 Soils

The dominant soils covering the project site are Capey-Clear Lake clay, Dibble-Los Osos loam, Altamont clay and Toomes stoney loam. The predominant texture of the surface soil is clay loam with moderate to high shrink-swell characteristics. The topsoil varies from 7 to 13 inches in thickness; the subsoil varies from 17 to 18 inches over a substratum layer of weathered bedrock, becoming harder with depth.

Within the region, these soils are commonly found in upland areas where slopes are 15% or more. The shallowness of the soils, low availability of water and steepness typically limit the soils' agricultural use to rangeland and dry farming. Depending on slope, the soils maintain an SCS Classification of 2.8, mostly being classified as 3 and above. Classification 1 is the most usable land while classification 8 is the least useable. In the upland area to the north and the east of Columbus Parkway, stones cover 10% to 50% of the surface area and the depth of bedrock is 5 to 10 inches. Rock outcrops are common on these steeper slopes.

Landslides and general slope instability present the most serious geologic impacts on the proposed project. Landslide deposits are abundant in the area. The slopes north and east of Columbus Parkway are underlain by extensive landslide deposits which extend vertically from the valley bottom to the base of the cliffs at the ridgetop. These deposits extend laterally from near I-80 toward the southeast for a distance of more than 7,000 feet. This region has therefore been designated as a visual and natural resource and is not to be developed. Numerous shallow soil slips (small movements of soil occurring under saturated conditions) are also evident.

The unstable conditions have been considered and appropriate land uses and development guidelines will help mitigate these conditions. The first guideline, as with the slopes to the north and east of Columbus Parkway, is to restrict development. Another step is to properly stabilize

the toe of the slopes where development is occurring through proper engineering and grading practices. In these areas, buildings will be required to have a minimum setback of 50 feet from the toe of the slope in order to assure a safe distance should a movement occur; or, if required, such landslides will be repaired. Many of the soil conditions will be resolved through the grading of the site by cutting away the unstable soils and reducing the severity of the slopes. It is critically important that the master grading plan and any geotechnical recommendations are followed. Soils and engineering reports will be prepared for the development of each parcel and should emphasize the resolution of problems of slope instability and potential foundation problems resulting from highly expansive soils. For further description of site soils and geotechnical conditions, please refer to the study completed by Harding Lawson Associates.

Soils Classifications ¹	Soils
Altamont clay, 30-70% slopes, eroded	VI
Clear Lake clay, 0-2% slopes	II
Clear Lake clay, 2-5% slopes	III
Dibble-Los Osos clay loam, 9-30% slopes	IV
Dibble-Los Osos loam, 2-9% slopes	III
Dibble-Los Osos clay loam, 30-50% slopes, eroded	VI
Toomes stony loam, 30-75% slopes	VIII

1.6.6 Seismicity

Only one fault zone, the Sulphur Springs Valley Fault, is known to traverse near the project area. This fault has been mapped as passing from northwest to southeast approximately at the base of the ridgetop cliffs northeast of Columbus Parkway. Another trace of the fault curves around the ridgetop in the northeast corner of the property. The age of the last movement on this inactive fault is unknown, although the State Fault Map shows it as pre-Quaternary (older than 2 million years). The project area lies in a seismically active area of Northern California. Although no active faults cross the property, earthquakes generated on nearby faults would cause ground shaking of various levels of intensity.

1.6.7 Drainage

The project area is within the 4,100-acre Lake Chabot watershed which is formed by the south and west-facing slopes of Sulphur Springs Mountain and includes two major streams, Rindler Creek and Blue Rock Springs Creek. Drainage is westerly under I-80 to Lake Chabot. The watershed is largely undeveloped grasslands. About 30% of precipitation currently falling on the site results in surface runoff. Lake Chabot is used primarily to regulate storm flows to the downstream areas of White Slough and the Napa River. Three separate sub-basins of the Lake Chabot watershed occur in the project area: the Rindler Creek drainage area in the north, the Blue Rock Springs Creek drainage area in the south, and the Turner Parkway area between the two creeks.

¹ Solano County Soil Conservation Service.



SOILS



DRAINAGE

Existing improvements in the Rindler Creek drainage area include two 6-foot culverts, which transition to a 10-foot by 6-foot culvert, and two 66-inch corrugated metal pipes (CMPs) under I-80, a box culvert crossing downstream and at Admiral Callaghan Lane and two 48-inch CMPs under Columbus Parkway at the intersection of St. John's Mine Road.

The Turner Parkway area drains to a natural channel which crosses I-80 to Rindler Creek in one 60-inch reinforced concrete pipe and three 30-inch corrugated metal pipes.

Blue Rock Springs Creek drains the largest basin within the Lake Chabot drainage area, with headwaters in Sulphur Springs Mountain. Although spring-fed, the creek is primarily a storm drainage channel, collecting surface runoff and carrying it to Lake Chabot. The creek crosses under I-80 at Admiral Callaghan Lane just north of the Redwood Street interchange through two 72-inch concrete pipes and two 60-inch CMPs. Upstream of I-80 between I-80 and Hanns Memorial Park, two 72-inch concrete pipes carry storm runoff underground through the developed portion of Blue Rock Springs Creek.

1.6.8 Circulation

Interstate 80 is the major regional freeway serving the project area, providing access to central Vallejo, Richmond, Oakland and San Francisco to the southwest ("westbound") and to Fairfield and Sacramento to the northeast ("eastbound"). I-80 is six lanes south of the Columbus Parkway/State Route 37 interchange (three lanes in each direction), and eight lanes (four lanes in each direction) north of the SR-37 (Marine World Parkway) provides regional access to Marin and Sonoma Counties to the west as well as to connecting routes to Napa County to the northwest. SR-37 is six lanes between I-80 and Fairgrounds Drive and two lanes west of Fairgrounds Drive.

Principal arterials adjacent to the project, as classified by the City of Vallejo, include Redwood Street and Redwood Parkway east of Admiral Callaghan Lane. Minor arterials include Columbus Parkway, Oakwood Avenue, Admiral Callaghan Lane and Fairgrounds Drive. Collector streets include Tennessee Street, Vervais Avenue, Skyline Drive, Lake Herman Road and St. John's Mine Road.

Existing traffic volumes are heavy on I-80, on the Redwood Street overpass of I-80 and on the nearby freeway ramps. Traffic volumes are relatively light on Columbus Parkway, Admiral Callaghan Lane and Lake Herman Road. I-80 traffic is predominantly westbound in the morning and predominantly eastbound in the evening. A secondary commute pattern within the general vicinity of the project is from eastbound SR-37 to westbound I-80 in the morning and from eastbound I-80 to westbound SR-37 in the evening. Existing development near the project area is primarily residential with higher volumes towards I-80 on-ramps in the morning and away from the I-80 off-ramps in the evening. The major traffic capacity problem in the Northeast Quadrant is at the intersection of Redwood Street and Fairgrounds Drive and the westbound I-80 freeway ramps.

1.7 EXISTING INFRASTRUCTURE

1.7.1 Sanitary Sewer

The project area will be served by the Vallejo Sanitation and Flood Control District. The existing wastewater treatment plant has a capacity of about 12.5 million gallons per day (mgd) and services about 6,000 acres of developed land. Improvements are being planned to expand treatment capacity by 4 mgd. Treated effluent is ultimately discharged through a deep water outfall in the Carquinez Strait. A 15-foot wide sewer trunk easement exists along Admiral Callaghan Lane north of Turner Parkway.

1.7.2 Natural Gas and Electricity

Natural gas and electricity are provided to Vallejo by the Pacific Gas and Electric Company (PG&E). PG&E is in the process of extending full service to the area known as "Vallejo Corners". There are two existing electrical transmission lines in the project area; however, the only distribution points are to Blue Rock Golf Course and across the back side of the Sulphur Springs Mountain ridge to residents along St. John's Mine Road.

1.7.3 Telephone

Underground facilities on Turner Parkway and Columbus Parkway will be necessary to complete service extensions to the project area. Conduit and cable extensions will be installed in utility easement along proposed streets.

1.7.4 Water

The project area will be served by the Water Operations Division of the Vallejo Public Works Department. Sources for water for the City of Vallejo include Cache Slough Diversion and the Barker Slough Diversion located southwest of Vacaville, Lakes Frey and Madigan, Lake Curry, and Lake Berryessa via the Putah South Canal. Vallejo expects to receive additional State Water Project entitlements from the Cache Slough Diversion. All water is treated at the source or at the Fleming Hills Treatment Plant located north of Lake Chabot.

There are two existing water tanks on the site. One east of Hunter Ranch has a present capacity one million gallons of potable water. An additional site has been located there and will be implemented with the construction of this project. The other tank site is located farther east near Columbus Parkway and has a capacity of one-half million gallons of untreated water for golf course and park irrigation.

1.7.5 Solid Waste

Vallejo Garbage Service provides solid waste disposal under a franchise granted by the City.

SECTION 2. NORTHGATE DEVELOPMENT PLAN

2.1 LAND USE AND DEVELOPMENT PLAN

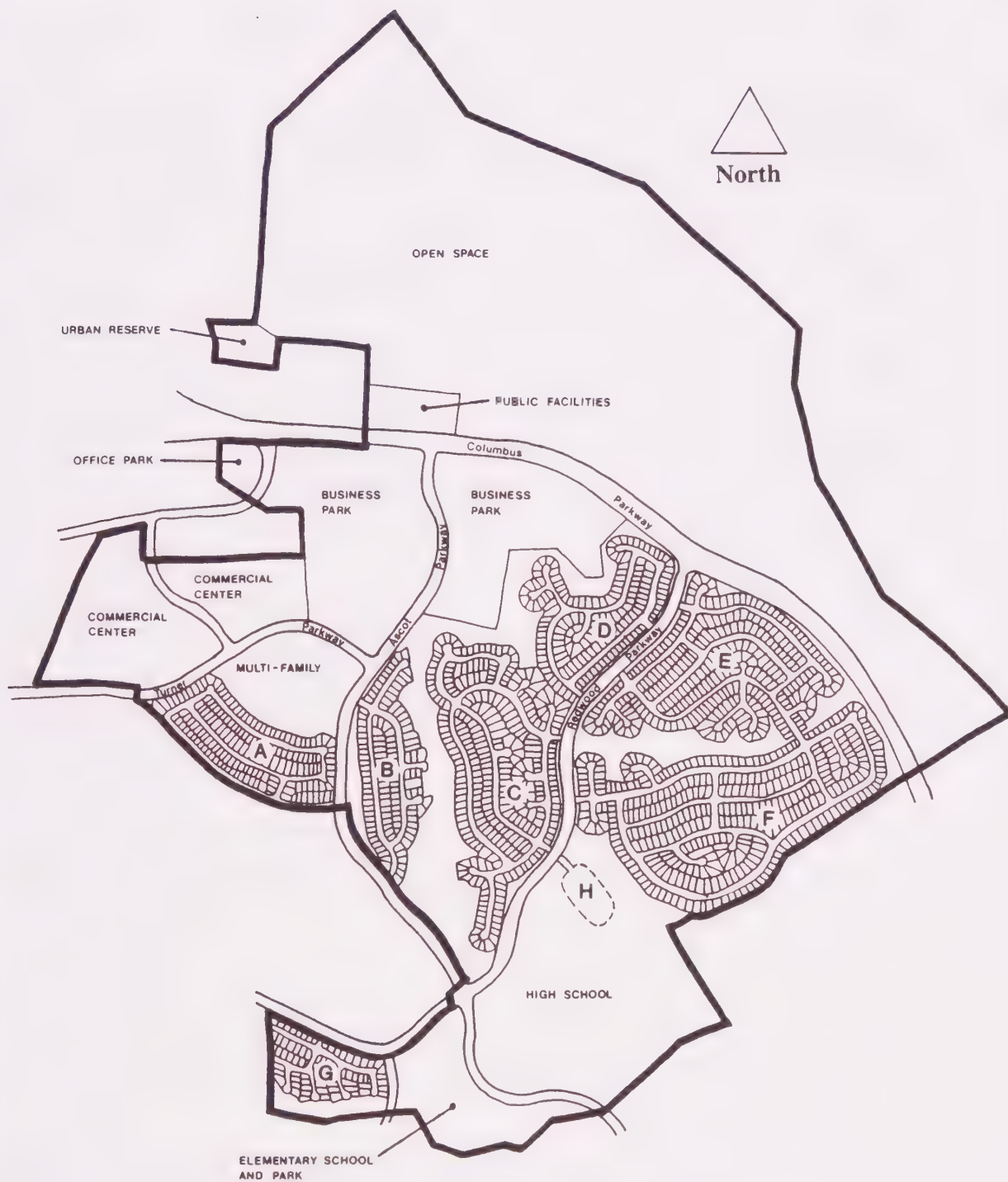
This Specific Area Plan proposes a planned mixed use development of housing, commercial, office, business, recreation and open space. Over 570 acres of the original Lundeborg Properties' 1,165-acre site have been retained as open space to buffer varied land uses, provide a visual amenity, and preserve the natural landscape. Within the project as a whole, there will be separate, identifiable business and residential communities.

LAND USE SUMMARY TABLE²

<u>LAND USES</u>	<u>ACREAGE TOTALS</u>
Office Park	4.1
Commercial Center	62.3
Business Park	105.9
TOTAL	172.3 ACRES
Low Density Residential	312.5
Medium Density Residential	26.3
TOTAL	338.8 ACRES
High School	40.0
Elementary School/Park	27.4
Fire Station	1.0
Park and Ride	3.0
PG&E Substation	6.0
TOTAL	77.4 ACRES
Undeveloped Open Space (north of Columbus Parkway)	421.8
Open Space (south of Columbus Parkway)	114.8
TOTAL	536.6 ACRES
Circulation	33.5
Urban Reserve	6.0
TOTAL	39.5 ACRES
<u>TOTAL</u>	<u>1,164.6 ACRES</u>

All figures are derived from a 1"=300' drawing, and are subject to appropriate tolerances of measurement.

² The amount of square footage of nonresidential uses shall not exceed 1,929,000 square feet unless additional environmental review is undertaken. If such review concludes that all traffic and other impacts from an increase in square feet can be mitigated, then the area for nonresidential uses may be increased to a maximum of 2,212,000 square feet, and suggested mitigation measures shall become conditions of the project.



Northgate Specific Area Plan

Vallejo, California

2.2 RELATIONSHIP TO THE VALLEJO GENERAL PLAN

Typically, the SAP is less intense in development than is shown on the General Plan. General areas of housing and an employment center are consistent between the two documents; however, there are specific variations between the proposed SAP and the existing plan.

The northerly 421.8 acres of the project area within the proposed SAP boundaries is presently designated "Rural Residential", permitting lots of 5 to 15 acres. The proposed SAP designates this area as "Undeveloped Open Space", and as a result will remain undeveloped.

The 6-acre parcel north of Columbus Parkway is designated "Urban Reserve". This designation will remain until such time as the adjacent properties (known as the NDS and Olson properties) are proposed for development. Then the 6-acre piece will be considered for development, and will undergo additional environmental review as required by the City.

A major portion of the project site is presently designated "Low Density Residential". The proposed SAP designates approximately 312.5 acres as "Low Density Residential" which is significantly less than under the present General Plan designation.

Presently the General Plan indicates a small area within the SAP site adjacent to Columbus Parkway as "Medium Density Residential". This would be expanded to 26.3 acres and relocated under the proposed SAP to the southwestern corner of Turner and Ascot Parkways.

The proposed SAP is compatible with the General Plan since significant portions of the Northgate site are being developed as commercial/retail and professional office and business park uses.

The Blue Rock Springs Creek Corridor is being maintained as open space which is consistent with the City's Plan. Approximately 422 acres to the north side of Columbus Parkway are being retained as open space and are not being developed. In combination with the 171.3 acres of open space between the various residential uses, significantly more open space than is presently shown in the General Plan will be provided.

Two school sites are proposed on the plan: one high school and one elementary school. Both will include the Blue Rock Springs Creek Corridor within their grounds.

2.3 CONSISTENCY WITH THE VALLEJO GENERAL PLAN'S GOALS AND POLICIES

The proposed Specific Area Plan is in substantial conformance with the goals and policies of the City. The amount and location of open space provided within the SAP will provide the buffer recommended by the General Plan. The location of the Low Density Residential neighborhoods will respect the existing neighborhoods by their location and design.

The location of the Medium Density Residential is placed so that it not only buffers the single family neighborhoods, but directs their traffic away from these same neighborhoods.

Locations of the Business, Office and Commercial areas are placed where they will have the fewest impacts on residential uses and traffic circulation, and where they will be the most successful for the economic development of Vallejo.

The street system has been designed to reduce the conflicts of the residents with the primary parkways throughout the project by not having residential units directly exiting onto these parkways and by minimizing the number of intersections. In addition, the interior residential streets are planned to avoid four-way intersections and oblique intersections. Bicycle and pedestrian routes will also be provided as required.

SECTION 3. THE OFFICE PARK LAND USE ELEMENT

The Office Park is located on Admiral Callaghan Lane above Columbus Parkway. The Office Park as proposed will contain 4.1 acres of both single and multiple user space. Its central location within the Northgate Plan will set the standard by which other developments within the Plan will measure themselves. The "Development Standards" establish architectural and landscape guidelines for the design and the construction within the Office Park.

The uses that will be allowed in the Office Park with an approved Planned Development Unit Plan:

- Administrative and professional services;
- Business equipment sales and services;
- Business support services;
- Communications services;
- Convenience sales and personal services;
- Eating and drinking establishments, if not described below;
- Finance, insurance and real estate services;
- Medical services;
- Personal services of functional community training for developmentally disabled;
- Research services;
- Retail sales associated with office uses;
- Day care facilities;
- Essential services.

The uses that will be allowed in the Office Park with an approved Major Use Permit:

- Eating and drinking establishments, with on- or off-site sale of alcoholic beverages;
- Participant sports and recreation;
- Major impact utilities and services.

No industrial or residential uses will be allowed.

No fast food restaurants or drive-up windows will be allowed.

The Office Park will be designed to accommodate midrise office buildings with landscaped surface parking. Maximum building height for office buildings will be 60 feet with the floor elevation of the last

occupiable floor not to exceed 45 feet. The FAR is not to exceed .32 for the gross acreage of office use with a buildout of approximately 57,150 square feet on the 4.1 acres.

SECTION 4. THE BUSINESS PARK LAND USE ELEMENT

The 105.9-acre Business Park, located east and west of the intersection of Columbus Parkway and Ascot Parkway, is designed to carry on the development's quality image established by the Office Park. The Business Park will also have the asset of excellent freeway access from Columbus Parkway.

The quality environment in the Business Park will be insured through development standards very similar to those established for the Office Park. Landscape easements will be required on all internal roads, as well as landscaping around the building perimeters and throughout the parking lots. Ascot Parkway will maintain a 25-foot wide landscape zone on both sides. A height limitation of 45 feet has been imposed. Overall, the visual image for either an employee, a neighbor, or a visitor will be one of well designed buildings in extensively landscaped sites.

Development in the Business Park is expected to be more diverse than the Office Park, which is a reflection of its more developable terrain and prominent visibility. Development sites will average 5 acres in size, but none less than 1 acre.

The uses that will be allowed in the Business Park with an approved Planned Development Unit Plan are the same as the Office Park. The following uses shall be allowed in the Business Park subject to Major Use Permits:

- Construction sales and services;
- Repair services;
- Wholesale storage and distribution;
- Light manufacturing and assembly;
- Automotive and equipment: sales/rental, light equipment.

No residential or heavy industrial uses will be allowed.

No fast food restaurants or drive-up windows will be allowed.

Any of the allowable uses which conducts all or part of its business outdoors on its site and/or which has outdoor storage shall screen all such activities from the public right-of-way and adjacent properties.

Although office buildings will be allowed, it is expected the character of the Business Park will tend towards large floor area lowrise facilities for clean R&D, distribution and warehouse space. The maximum buildout of the 105.9-acre Business Park is approximately 1,476,000 square feet with a FAR not to exceed .32 for the gross acreage. The landscape guidelines will insure that all parking lots will be landscaped and that a landscaped buffer will screen them from the street.

SECTION 5. THE COMMERCIAL LAND USE ELEMENT

The Commercial Center site of 62.3 acres is located north of Turner Parkway and next to Vallejo Corners Shopping Center.

It is hoped that the uses within the commercial area include well-known retailers, a department store, and provide goods and services not currently not available in Vallejo. It is also hoped that neighborhood services be kept to an acceptable minimum in order to encourage the regional nature of the site. Neighborhood services should be limited.

Development standards established for the Business Park will apply to the commercial area as well. The FAR will not exceed .25 for the gross acreage, with a total buildout not to exceed approximately 678,000 square feet of commercial/retail space.

Primary access for the site will be from the proposed extension of Plaza Drive. The Center will be designed to minimize the access along Turner Parkway in order to reduce the impacts on the adjacent residential neighborhoods. All perimeter streets will be adequately landscaped in order to maintain a quality streetscape as well as compliment the landscapes of adjacent properties.

The uses that will be allowed in the Commercial Center with an approved Planned Development Unit Plan:

- Administrative and professional services;
- Animal sales and services (indoor);
- Building maintenance service;
- Building supplies;
- Business equipment sales and services;
- Business support services;
- Communications services;
- Convenience sales and personal services;
- Cultural exhibits;
- Eating and drinking establishments, if not described below;
- Fast food restaurants in attached buildings;
- Finance, insurance and real estate services;
- Food and beverage retail sales, if not described below;
- Garden supplies;
- Laundry services;
- Lodging;
- Participant sports and recreation (indoor);
- Personal services, functional community training for the developmentally disabled;
- Personal services, general;
- Postal services;
- Repair services, consumer;
- Retail sales;

Wholesale sales when associated with retail sales;
Day care facilities;
Essential services.

The uses that will be allowed in the Commercial Center with an approved Major Use Permit:

Eating and drinking establishments, with on- or off-site sale of alcoholic beverages;
Food and beverage retail sales, with on- or off-site sale of alcoholic beverages;
Participant sports and recreation (outdoor);
Major impact utilities and services.

Fast food restaurants (maximum of two) on freestanding pads under the following conditions:

- a. One fast food restaurant upon written confirmation from the project sponsor to the City that the Commercial Center has obtained written commitments for location in the Center from the following firms and/or their equivalent:

A Fry's grocery store;
A Long's drug store;
A Home Club home improvement store; and
A Ross and/or Marshall's retail clothing store.

- b. A second fast food restaurant upon occurrence of any one of the following events:

Verification from the project sponsor to the City that the project sponsor has secured a retail tenant for 80,000 or more square feet;

Verification from the project sponsor to the City that the project sponsor has secured: two retail tenants of 15,000 square feet or more and retail tenant of 10,000 square feet or more; or a combination of three retail stores totaling a minimum of 40,000 square feet; or

Verification from the project sponsor to the City that the project sponsor has secured a "dinner house" restaurant within the Northgate project of least 6,000 square feet and including a full bar/cocktail lounge.

The fast food restaurants shall be consistent with the rest of the center in terms of building design, materials, colors, details; landscaping; pedestrian paving surfaces; signage; street and pedestrian furniture. They shall not be allowed to maintain their corporate architecture.

Light automotive services under the following conditions:

- a. It is limited to tune-ups, lubes and oil changes, muffler installations, brake repairs, and wheel alignments.

- b. It is allowed in only area of the Center in one building, and no freestanding pads.
- c. It is conducted entirely indoors with no overnight storage of vehicles.
- d. The building shall be identical to the remainder of the approved Center in terms of building design, materials, colors, details; landscaping; pedestrian paving surfaces; signage; and street and pedestrian furniture.
- e. The use shall not visually impact adjoining uses and/or properties. Service bays shall be screened from adjoining uses and/or properties by the orientation of the building and/or landscaping.

A gas station and car wash (maximum of one each) under the following conditions:

- a. They are to be located on the east side of Plaza Drive only.
- b. They are to be consistent with the Center in terms of building design, materials, colors, details; landscaping; pedestrian paving surfaces; signage; street and pedestrian furniture.

No residential, except lodging, or industrial uses will be allowed.

Commercial will typically be one or two stories with landscaped surface parking.

SECTION 6. RESIDENTIAL NEIGHBORHOOD ELEMENT

One of the significant design features of the Northgate residential area is its topography and the variety of open space and recreational opportunities that surround it. Varied elevations can provide interesting street patterns, hilltop views and a variety of housing types and site treatments, i.e., split level homes, terraced homes, and terraced gardens, etc.

Two residential types of development are proposed for the Plan. Low density detached single family homes on their own lots with various lot sizes and medium density residential housing. In addition, community education and neighborhood recreation facilities will be permitted.

Within the low density detached single family category different types of housing will be available to the home buyer. These will include a range of affordability, density, style and size. The specific types of housing to be provided will be determined by the market demand.

6.1 LOW DENSITY RESIDENTIAL NEIGHBORHOODS

It is the intention of the Northgate Plan to create within the community eight³ residential neighborhoods, totaling 312.5 acres, that have their own sense of identity and entry. By requiring through development guidelines well-designed entry gates, perimeter landscaping, strong street patterns, and a continuity of architectural styles, the essence of these residential neighborhoods can be reinforced.

These residential neighborhoods are defined on the northeast by Columbus Parkway and the Sulphur Springs Mountain's ridgeline beyond. To the east and south lies Blue Rock Springs Park and Golf Course and Blue Rock Springs Corridor and linear park. The Blue Rock Springs Golf Course may be extended into the project area with the approval of the City based on input from the Vallejo Golf Club. One residential neighborhood will buffer the existing residential neighborhood to the south from the proposed uses along the extension of Turner Parkway.

Each of the low density residential neighborhoods will be completely surrounded by landscaped easements, street frontage and open space. In addition, 114.8 acres of open space separate the neighborhoods from each other. This open space provides, by the use of topography, slope banks and planting, buffers and screens between the neighborhoods as well as an open space/greenbelt spine.



Prototypical Single Family Lot

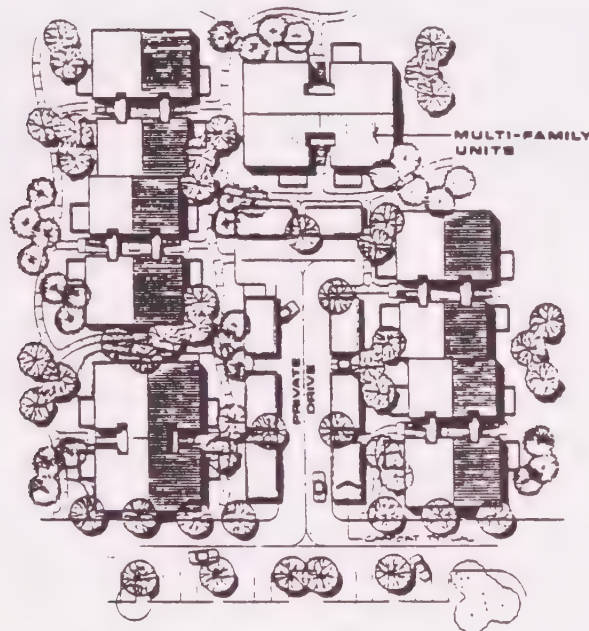
³ The eighth neighborhood, "H", shall be permitted as follows. The number of units shall be determined by the City and shall not exceed 48 units. Further, the configuration and total buildable land of the golf course area shall be subject to the acceptance of the Vallejo Golf Club Board of Directors and/or golf course architect, and no portion of Neighborhood H shall be immediately adjacent to the proposed new high school. The design for this area shall be subject to review and approval by the City.

The low density residential area will be entirely single family homes. The maximum number of units may be 1,524, although the final number of units may be less depending on the configurations of future approved subdivisions by the City. Many of the lots will have direct golf course frontage or open space frontage. Each lot will have a minimum of 3,600 square feet of usable space. This minimum usable space is defined as buildable and flat usable space. Minimum lot size is 4,500 square feet. Lots may be as large as 10,000 square feet in some of the neighborhoods. All residences will be accessed from interior residential streets. There will be no residential frontage on Columbus Parkway or any of the major parkways throughout the property.

6.2 MEDIUM DENSITY RESIDENTIAL NEIGHBORHOOD

The one 26.3-acre site for medium density housing is located at the southwestern corner of Turner Parkway and Ascot Parkway. The development area will be separated from the traffic by a 25-foot landscaped easement in addition to the remaining ROW. A planted embankment (as required) within the easement will give the residential area privacy as well as a buffer from the street noise. As noted in "Low Density Housing Neighborhoods" this housing type will also have a well designed entry gateway to help reinforce the neighborhood's identity.

Proposed housing types may include attached townhouses, condominiums and/or apartments. Based on a density of 16.4 units per acre as specified in the Vallejo General Plan there will be a maximum of 432 units on the site. Due to the physical size of the site and as shown on the Plan this parcel may be subdivided at a later time into more than one medium density development. As proposed through the total number of medium density residential units will not exceed the proposed SAP unit count.



Prototypical Medium Density Housing

The medium density units will typically be two stories with individual entries and surface parking. For the most part, the units will share common walls. These units will be clustered together with a variety of balconies, overhangs, recessed windows, sun porches and stairways. These elements will help break up the mass of the buildings and add interest to the clustered housing development.

Any proposed project shall require a Major Use Permit.

SECTION 7. URBAN RESERVE ELEMENT

The designation on this 6-acre parcel shall remain until such time as the adjacent properties (known as the N.D.S. and Olson properties) are proposed for development. Then this parcel shall be considered for development, and shall undergo additional environmental review as required by the City.

SECTION 8. PUBLIC FACILITIES AND SERVICES ELEMENT

8.1 CONCEPT

Within the Northgate Specific Area Plan, the City will require that existing public facilities within the community be extended into the Plan area. These facilities will include police and fire service, schools, parks, and a Park and Ride lot as well as the normal public health requirements such as utilities. The general extent and locations of these facilities is shown within this SAP and has been coordinated with the City and other public facilities.

8.2 FIRE PROTECTION

Fire protection will be provided by the City of Vallejo. A one-acre site for a fire station will be dedicated north of Columbus Parkway next to the park and ride lot. In addition, the development will have the following:

- a. a water supply sufficient to deliver required fire fighting flow and pressure; and
- b. hydrants, water mains, hydrant markers, paved access for emergency vehicles, clearly marked streets and adequate building identification.

8.3 POLICE

As required by the City, police services will be extended into this area.

8.4 ELEMENTARY SCHOOL

A 27.4-acre site will be dedicated for a joint elementary school and park. The site is located at the intersection of Redwood Parkway and Ascot Parkway, directly adjacent to the Blue Rock Springs Corridor. The central location provides easy access from the surrounding neighborhoods, including the Northgate community. Bike paths and pedestrian trails will ink the school/park site to the Blue Rock Springs Corridor. The school is presently being designed under the jurisdiction of the Vallejo City Unified School District.

8.5 HIGH SCHOOL

A high school site will be purchased by the School District through a Mello-Roos District. The site shall yield a minimum of 40 acres to meet State and VCUSD standards. The site is at the corner of Ascot Parkway and Redwood Parkway. The site is to be combined with the Blue Rock Springs Corridor and linked to this linear parkway by walks and bikeways.

Management and maintenance of this joint property will be by the School District and the Greater Vallejo Recreation District (GVRD).

8.6 PARKS

The site for a community park which is located adjacent to the elementary school will be dedicated as part of the school site to GVRD. It will be designed under the jurisdiction of GVRD.

8.7 PARK AND RIDE FACILITY

In keeping with the local and regional concern about traffic congestion, a 3-acre park and ride facility has been planned for the intersection of Columbus Parkway. This facility will be landscaped to mitigate visual impacts, but will still allow surveillance for security persons from Columbus Parkway. Bicycle racks will be provided.

SECTION 9. CIRCULATION ELEMENT

9.1 CONCEPT

The proposed SAP envisions that the road and street pattern will be an extension of the current parkway system in place by the City. Connections to Turner, Ascot, Redwood, and the expansion of Columbus Parkway will all reflect the City's desire for parkways as the major roadway collector system. Where possible the Plan will incorporate and expand the concept of landscaped rights-of-way by including additional landscape easements along the parkways.

As discussed within the residential sections each of the neighborhoods will each have their own entry gateway and internal circulation. The traffic generated in one neighborhood will not be directed through another neighborhood.

With the reduction of the Business and Office Parks' FAR (floor area ratio), the reduction in the number of residential units, and the revisions within the new plan, traffic circulation will be significantly altered. Therefore, mitigation measures included in the revised project are new roadways, provision for parking and transit bus stops. Foremost is the relocation of Turner Parkway at Foothill Drive to help protect the Hunter Ranch area from non-residential traffic.

Pedestrian sidewalks are provided on Ascot Parkway, Redwood Parkway, Columbus Parkway, Turner Parkway and the residential streets. Signed and striped continuous bike lanes are provided on major arterials, while a separate bikepath would be provided on Columbus Parkway. A Transportation System Management (TSM) program is encouraged to be developed, and a 3-acre park and ride lot is included in the project, as part of the TSM program.

Recommended mitigations include signalizations of several intersections, adding left turn pockets, and some right turn lanes, where heavy turn volumes are identified. All the intersections studied could be mitigated to a level of service "D" or better. Other areawide recommended mitigation measures include additional parking spaces at the Blue Rock Springs Park, the extension of a fixed bus route into the project area, and improvements to the Transportation System Management program outlined in the project. Additionally, freeway improvements will be required as a part of increased regional traffic demand, such as added through lanes. These measures are being more completely studied as part of MTC's I-80 Corridor Study.

The traffic mitigation measures described in Figure IV-1 of the Supplement volume of the FEIR⁴ shall be incorporated into the project and shall be the responsibility of the project sponsor. Exceptions to these mitigation measures may be made if the City, Caltrans and/or project sponsor can demonstrate traffic impacts can be lessened further using alternative means.

9.2 EXISTING STREETS

Where possible impacts on the existing street pattern have been minimized. By reorganizing the Plan so that low density residential lots are adjacent to the existing Hunter Ranch development housing, traffic impacts are reduced.

9.3 PLANNED STREETS

Plaza Drive will be realigned, widened to four lanes and extended to Turner Parkway. This extension will be used to service both the proposed commercial and office developments. By doing this, access from Turner will also be more specifically controlled. The final street standard shall depend on the development on either side and the points of access to serve such development.

Columbus Parkway will be widened to meet the City's design standards for the remainder of the street. By utilizing the current ROW the impacts to the community will be reduced. Columbus Parkway provides the most logical access to I-80 and to the rest of Vallejo to the west.

⁴ The traffic section of the Supplement to the FEIR is entitled "Northgate Specific Area Plan, Traffic and Transportation Element" by DKS Associates, dated June 1988.

The extension of Ascot to Columbus is an additional north/south route. As a major collector, it serves as a primary access to the Business Park and some of the Low Density Residential. Truck traffic will be limited to servicing the Business Park. Bicycle and pedestrian access are provided within the ROW. At the intersection of Ascot with Columbus the entrance to the park and ride will occur.

Most of the residential neighborhoods will be serviced by Redwood Parkway, a four-lane road with a planted median and bike path. A 50-foot wide landscape and slope easement on both sides will be provided in addition to the ROW. The street loops through the residential area, extending from the existing terminus of Redwood Parkway. No residential driveways will access directly onto Redwood Parkway and signs will be posted to restrict truck traffic.

The extension of Turner Parkway from Admiral Callaghan Lane will service the Commercial Center and the Medium Density Residential. This will also be a limited access parkway, fully landscaped and with a median.

Collector streets are provided for the Business Park. In the Business Park, the streets are designed to have four lanes to accommodate anticipated traffic volumes.

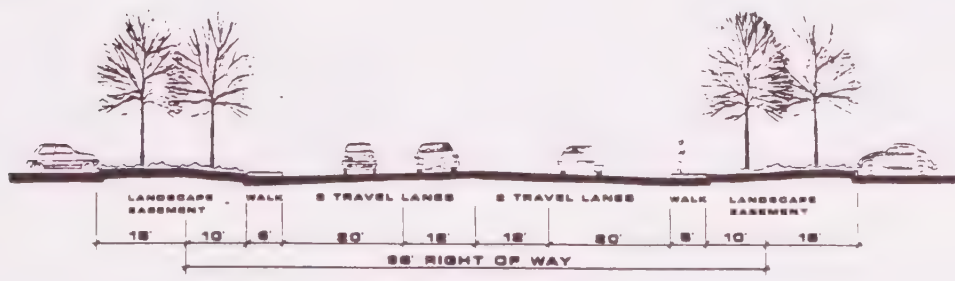
The Commercial Center site will have access from Turner Parkway and will have its primary access from Plaza Drive. Circulation to the different users within the Center will be done internally.

Due to the realignment of Turner and the abandonment of portions of the existing ROW, the remaining street that services residential "A" will have a "Not a Through Street" sign posted at the access/entry point. The street's name shall not be "Turner".

Residential streets are proposed to be typically 32 feet wide within the proposed 50-foot wide street right-of-way and designated with parking on one side only. In addition, four-foot sidewalks will be placed on both sides of the street.

Driveway access onto major streets shall be limited.

Typical Section Through Plaza Drive:



Typical Section Through Ascot Parkway:



Typical Section Through Redwood Parkway:



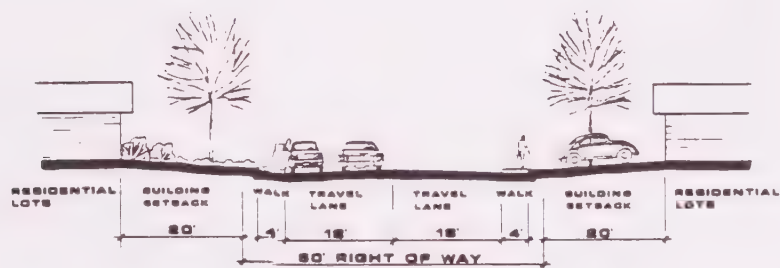
Typical Section Through Turner Parkway:



Typical Section for Residential Collector Street:



Typical Section for Residential Street:



9.4 PEDESTRIAN PATHS AND TRAILS

Bike lanes and barrier free pedestrian crossings will be provided on all major streets. Bus stops and bus turnouts with bus passenger shelters will also be incorporated at appropriate intersections as deemed necessary by the City of Vallejo and Vallejo Transit. For specific street design, signalization and bus stop locations, please see "Public Improvement Standards". In addition to the relocation of the existing Turner Parkway to its new alignment a pedestrian/bicycle corridor will be improved along its old location to provide a trail connection from the low density residential neighborhoods to the Commercial Center.

SECTION 10. OPEN SPACE ELEMENT

10.1 CONCEPT

Within the project area there are 536.6 acres of open space. This includes the 114.8 acres of open space buffer between the Low Density Residential, the 421.8 acres of open space north of Columbus Parkway and the Blue Rock Springs Corridor to the south. This does not include the expansion of the Blue Rock Springs Golf Course into the project area.

10.2 CHARACTERISTICS

The primary design goal is to retain an open rolling open space landscape that is consistent with the existing hillsides. Careful grading practices will make a harmonious transition from the housing and the road grades to the rolling open space. Planting in strategic areas will create entries into the residential neighborhoods (see "Landscape Concept"), help screen existing power lines and water tanks, and provide erosion control while protecting views for residential lots.

Rindler Creek will be moved to north of Columbus Parkway and will be replanted with native riparian plants consistent with the requirements of the U.S. Army Corps of Engineers and the California Department of Fish and Game to become a transition from the parkway to the open space (see "Drainage"). This will also help to protect this riparian zone from the effects of polluted road runoff.

Where the trail(s) winds through the interior open space it shall be designed and integrated with the 25-foot irrigated landscape buffer and 25-foot disced fire zone or 50-foot disced zone for lots adjacent to the open space. This trail(s) will be held to a minimum to help insure the privacy and security of the adjacent homeowners, reduce the possibility of grass fires, and to help minimize erosion. The trail(s) will also be designed to serve as maintenance access for fire management.

Typically, maximum slope of disturbed areas within the open space shall be 2:1 with vegetation as recommended by the City's Landscape Guidelines for Hillside Development. In unusual circumstances, steeper slopes may be allowed and desired upon approval from the City.

10.3 OPEN SPACE DISPOSITION

The large open slope north of Columbus Parkway shall be temporarily retained as private property and serve as undeveloped Open Space until an agreed upon disposition of the property can be made. Currently a number of options are possible. These are:

- a. Retention of the property by Vallejo Development Company or its successor-in-interest.
- b. Dedication of the property to GVRD.
- c. Donation or sale of the property to a public/private institution who will manage and maintain the property.

The remaining open space that occurs between the land uses south of Columbus Parkway will also have a number of options as regards its disposition. These are:

- a. Dedication of the property to GVRD.
- b. Establishment of a Homeowners Association to maintain these lands.
- c. Donation or sale of the property to a public/private institution who will manage and maintain the property.

The portion of Blue Rock Springs Corridor adjacent to the elementary school site will be dedicated to GVRD and the portion adjacent to the high school site will be sold to GVRD. In both cases the management and maintenance of the properties will be by GVRD.

Certain agricultural uses, such as grazing, shall be permitted in the permanent open space areas where feasible and where allowed by the City of Vallejo.

SECTION 11. PUBLIC INFRASTRUCTURE ELEMENT

11.1 WATER

Water will be supplied to the Northeast Quadrant by the City of Vallejo via the Trans-Vallejo pipeline system. The project area will be divided into three pressure zones with booster pumps providing services to the upper pressure zones (those areas above the 160-foot elevation). The water system will include three reservoirs, booster pumps, and water mains ranging in size from 8-inch to 16-inch diameter.

11.2 SEWER

Sewerage within the proposed development would meet the requirements of the Vallejo Sanitation and Flood Control District. It will be collected to an existing 24-inch sanitary sewer along Admiral Callaghan Lane. Capacity at the existing waste water treatment plant is adequate to meet the requirements of the project area.

11.3 STORM DRAINAGE

The project area drains into three separate drainage basins that cross under I-80 and ultimately into Lake Chabot on the westerly side of I-80. It is proposed to drain the site into these existing storm drainage systems as per the requirements of the Vallejo Sanitation and Flood Control District.

The southerly portion of the project drains into Blue Rock Springs Creek. It is proposed to leave the creek in its natural state as it flows through the project. A buffer zone has been established to maintain the aesthetics of the channel.

The northerly portion of the project drains into Rindler Creek, Rindler Creek is a natural channel that is quite steep and high velocities are possible. Rindler Creek is presently subject to erosion and contributes sediment to Lake Chabot during periods of high storm runoff. It is proposed to realign Rindler Creek on the north side of Columbus Parkway and flatten the slope to reduce the velocities. The realigned creek would be lined with rocks and contain drop structures to accommodate the flatter slopes.

The middle of the project drains to Turner Parkway. The Turner Parkway drainage basin is a small area between Rindler Creek and Blue Rock Springs Creek that is proposed to be drained into a new underground pipe system. The new system will connect to the existing pipes crossing at I-80 at the intersection of Admiral Callaghan Lane and Turner Parkway. It is proposed to expand the storm drainage system under I-80 to accommodate the increased runoff created by development.

11.4 UTILITIES

All residential and office/business uses will be provided with utilities as required by the City.

11.5 STREETS

All streets will conform to the City of Vallejo's construction standards for street design and construction.

11.6 STREET LIGHTING

All parkways and residential streets will meet the requirements of the City of Vallejo's street lighting standards.

11.7 GRADING

The objectives of the preliminary grading plan are to maintain the hillside character and appearance of the site, provide views from the homes and offices, and achieve a balance of cut and fill on the site.

The existing land forms have been a major design consideration and a primary factor in the aligning of roads and defining land uses. On the remainder of the site, grading is necessary to create buildable lots and to provide an adequate and safe roadway network. The proposed grading also serves to mitigate difficult soil conditions and eliminate hazardous landslide areas. The City's Hillside Standards for cut and fill have been used as a guide; however, in certain areas those standards have been exceeded to meet the above objectives. This SAP will establish those specific guidelines required for the property and will use the standards as a guide.

The placement of roadways was the major design constraint that determined the amount of grading required. Due to the rolling hillside landforms, it was not possible to align roads entirely along existing contours and at the same time adhere to all accepted street design standards. The existing topography rises and falls in too short a distance for roadways to continually follow existing terrain.

Improvements have, however, been designed to conform with the terrain to the greatest extent possible. The basic landforms have been generally maintained in that the highest existing points on the site remain the highest points and, likewise, the existing valley correspond to the lowest points on the site. The two highest points on the property will be recontoured and will remain in the open space, wherever possible.

to minimize grading and to avoid straight cuts through the hillside. Long rows of terraced lots were avoided by providing curvilinear streets and cul-de-sacs.

Where topography must be changed various contour grading techniques will be employed to allow the affected area to blend in with the natural slopes. Cut and fill slopes will be constructed with rounded corners to eliminate sharp angles of slopes at the contour intersections. Contour grading will also be utilized to create rounded, undulating slopes between street levels and building pads, and in transition areas from natural slopes to graded slopes. Slope gradients will vary, where feasible, to produce a more natural appearance.

Because the project is located in a hillside area, views are a primary amenity of the site. Building sites are terraced up the hillside in order to maximize the off-site views of the golf course, southwest toward the downtown area of Vallejo and north to Sulphur Springs Mountain ridgeline. Elevation changes between back-to-back lots are a result of minimizing the overall cut and retaining as much of the natural landform as possible.

No cutting is proposed on the north side of Columbus Parkway except as necessary to relocate Rindler Creek and to provide the public facilities.

Implementation of the development plan as proposed in this SAP will require approximately 8.5 million yards of earth to be moved. Excess cut from the single family residential areas is being used to create flatter pads within the Business area. Flat pads are necessary to provide adequate truck circulation, parking, and to accommodate the larger buildings occupied by business uses.



SCHEMATIC GRADING

SECTION 12. IMPLEMENTATION

Implementation of the concepts and policies set forth in the Northgate Specific Area Plan address several of the concerns of the City for the community's development. These include:

- a. Basic Goals and Policies.
- b. Development Policies and Standards.
- c. Long Term Management of Facilities.
- d. Enforcement Standards.
- e. Public Facilities and Services Financing Plan.

A discussion of these concerns follows.

12.1 BASIC GOALS AND POLICIES

The Northgate SAP is founded in the goals and policies established by the broader framework of the Vallejo General Plan. This Specific Area Plan endeavors to be consistent with its policy document in both to the approach of the land plan itself and the overall theme of the community that will be created. It is intended to be developed as an integral part of the community.

12.2 DEVELOPMENT POLICIES AND STANDARDS

Development within the Northgate SAP is intended to conform to an overall theme that is established within this Specific Area Plan, Development Standards and City Policies.

12.3 PHASING PROGRAM

The concept of the Phasing Plan is to build out the commercial, office, business and residential uses in orderly, balanced increments. Each land use follows a slightly different time frame. For instance, the residential development will be built out over a seven year period, the business/commercial will be built out over an eight year period, and the office will be built out over a ten year period.

As proposed in this SAP the intent of the developer is to initially begin construction of the employment base and then begin the residential component.

Even though there are slightly different time frames, each phase provides for the development of some office, business, commercial and residential.

Phase I:

Begin Commercial Center.

Utilize Columbus Parkway as primary access.

Build Plaza Drive.
Extend Ascot Parkway.
Construct intersection of Ascot Parkway and Columbus Parkway.
Extend Redwood Parkway.
Relocate Turner Parkway.
Widen and improve Turner Parkway.
Begin the low density housing neighborhoods.
Build Fire Station.
Build water tanks.
Begin grading of all neighborhoods and land uses.
Begin medium density housing.
Improve Admiral Callaghan Lane at Cooke property.⁵

Phase II:

Complete the internal road system.
Build the low density housing.
Complete medium density housing.
Begin Business Park.

Phase III:

Build out the remaining low density housing.
Develop private trails in the low density areas.
Begin Office Park.
Build park and ride.

12.4 ENFORCEMENT OF STANDARDS

12.4.1 Conditions, Covenants, and Restrictions (CC&Rs)

Within each neighborhood specific controls will be placed on each lot to guide the builder and homeowners during the construction of the neighborhood and during subsequent changes in the future. These CC&Rs are part of the purchase of the lot and are legally enforceable.

⁵ With reference to the completion of Admiral Callaghan Lane by the project sponsor, it is agreed and understood that the project sponsor's obligation to complete Admiral Callaghan Lane during Phase I is subject to the either:

- a. The project sponsor being able to obtain all necessary permanent and construction easements from the Melvin Cooke property; and/or
- b. The City acquiring the necessary permanent and construction easements from the Melvin Cooke property by exercising its right of condemnation over said property.

12.4.2 Zoning

The zoning for the project area will be changed to MUPD, Mixed Use Planned Development. Once this zoning is in place, the SAP will serve as the Planned Development Master Plan for the project and as the zoning requirements and standards.

12.5 FUNDING OF CAPITAL IMPROVEMENTS AND PUBLIC SERVICES

12.5.1 Financing

The financing for the project's construction and maintenance will generally take four forms: private funds, assessment districts, development fees and revenues, and homeowners' association fees. Each of these types is briefly explained below. Each project facility and land use is listed along with the proposed type of construction financing.

Private:

The developer will secure his or her own funds without any involvement or commitment from the City of Vallejo.

Assessment District:

This district will be similar to others formed by the City of Vallejo for other residential projects. It will insure the installation and maintenance of required infrastructure to serve the project. Only those who benefit from this infrastructure will be required to pay for it through the district.

Development Fees and Revenues:

The City of Vallejo charges a number of fees during the development process. These fees help mitigate the costs of providing new services to new development. In addition, new projects create revenues for the City through sales and property taxes. These revenues also support the expansion of services.

Community Facilities District (Mello Roos):

Development fees can be augmented by the formation of a Community Facilities District to assist in the costs of the schools, water and sewer improvements. Such a district would probably encompass the entire Specific Plan area.

12.5.2 Construction Responsibility and Maintenance Responsibility

See table on following pages.

FACILITY/ USE	CONSTRUCTION RESPONSIBILITY	CONSTRUCTION FINANCING	MAINTENANCE RESPONSIBILITY	MAINTENANCE FINANCING
Residential Low Density	Developer	Private	Private	Private
Residential Med. Density (owned)	Developer	Private	Homeowners Association	Homeowners Association
Residential Med. Density (rental)	Developer	Private	Private	Private
Office Park	Developer	Private	Private	Private
Business Park	Developer	Private	Private	Private
Commercial Center	Developer	Private	Private	Private
Schools	VCUSD	Mello-Roos	VCUSD	VCUSD
Park	GVRD	Development Fees	GVRD	GVRD
Fire Station	Fire Mitigation Fee	Fire Mitigation Fee	City	City
Water (on-site)	Developer	Assessment District	City	City
Water (off-site)	Developer	Assessment District	City	City
Sewer (on-site)	Developer	Assessment District	VSFCD	VSFCD
Sewer (off-site)	Developer	Assessment District	VSFCD	VSFCD
Storm Drain (on-site)	Developer	Assessment District	VSFCD	VSFCD
Storm Drain (off-site)	Developer	Assessment District	VSFCD	VSFCD

Rindler Creek	Developer	Assessment District	VSFCD	VSFCD
Blue Rock Springs Corridor	GVRD	GVRD	GVRD	GVRD
Streets	Developer	Assessment	City	City
Trails	Developer	Private	GVRD	GVRD
Open Space south of Columbus	Developer	Private	GVRD	GVRD
Open Space north of Columbus	NA	NA	GVRD	GVRD
Open Space between residential and business	Developer	Developer	Undetermined	Undetermined

SECTION 13. IMPLEMENTATION PROCESS

The steps within the implementation process may change if the City determines such change is in the best interest of the City and is consistent with local ordinances.

Amendment of the General Plan:

The Land Use Map will be changed to reflect potential development scenario.

Adoption of SAP:

The SAP is designated as the Master Plan of the Planned Development district.

Adoption of Zoning Changes:

The Master Plan provides the zoning standards which are comprehensive illustrations of the intent and purpose of the proposed project (Development Standards).

Approval of Subdivision Maps:

A general final map will be created to clarify the zoning districts of residential areas, Office Park, Business Park, and Open Space.

Approval of Improvement Plans:

Based on the Master Plan, construction drawings for general grading and the infrastructure (streets and utilities) will be completed.

Approval of Subdivision Maps:

At this stage, each parcel or project will be reviewed for its conformation to the Master Plan. This stage is also considered a design review. In the Office Park and Business Park, lot lines may be established on a parcel to parcel basis, provided the Master Plan Standards are met. In the residential areas, tentative maps are processed and reviewed to insure developments meet Master Plan standards.

Unit Plan Approval:

Detailed site plans and architectural plans are required; including, but not limited to, soils study, preliminary grading plan, landscape plans, architectural plans and elevations.

Building Approval:

Require detailed construction and engineering drawings. The project will be reviewed for its conformation to the approved Unit Plan or tentative map, City ordinances, Title 24 and the Uniform Building Code and City standards.

APPENDIX A:

DEVELOPMENT STANDARDS

NORTHGATE SPECIFIC AREA PLAN

TABLE OF CONTENTS

SECTION 1.	INTRODUCTION.....	A- 1.
SECTION 2.	PUBLIC IMPROVEMENT GUIDELINES.....	A- 1.
2.1	Scope of These Guidelines.....	A- 1.
2.2	Road Standards.....	A- 2.
2.2. 1	Columbus Parkway.....	A- 2.
2.2. 2	Ascot Parkway.....	A- 2.
2.2. 3	Turner Parkway.....	A- 3.
2.2. 4	Redwood Parkway.....	A- 5.
2.2. 5	Plaza Drive.....	A- 6.
2.2. 6	Business Park Collector Road.....	A- 8.
2.2. 7	Residential Collector Streets.....	A- 9.
2.2. 8	Residential Streets.....	A-10.
2.2. 9	Residential Cul-de-sacs Serving 20 Homes or Less.....	A-11.
2.2.10	Transit (Bus) Stops.....	A-12.
2.2.11	Landscape Maintenance Assessment District....	A-13.
2.3	Open Space.....	A-13.
2.3.1	Trails.....	A-13.
2.3.2	Erosion Control.....	A-13.
2.3.3	Fire Management.....	A-13.
2.3.4	Wildlife Management.....	A-13.
2.3.5	Rindler Creek.....	A-14.
2.3.6	Blue Rock Springs Corridor.....	A-14.
2.3.7	Wildlife Habitat.....	A-15.
SECTION 3.	PRIVATE DEVELOPMENT STANDARDS.....	A-15.
3.1	Low Density Residential.....	A-15.
3.1.1	Primary Use Permitted.....	A-15.
3.1.2	Purpose and Intent.....	A-15.
3.1.3	Auxiliary Uses Permitted.....	A-15.
3.1.4	Site Development Standards.....	A-16.
3.1.5	Required Landscaping.....	A-18.
3.1.6	Landscape Standards.....	A-18.
3.1.7	Grading Standards.....	A-18.
3.1.8	Architectural Standards.....	A-18.
3.2	Medium Density Residential.....	A-19.
3.2.1	Primary Uses Permitted.....	A-19.

3.2.2	Purpose and Intent.....	A-19.
3.2.3	Auxiliary Uses Permitted.....	A-19.
3.2.4	Site Development Standards.....	A-20.
3.2.5	Grading.....	A-21.
3.2.6	Required Landscaping.....	A-22.
3.2.7	Architectural Standards.....	A-22.
3.3	Office Park.....	A-23.
3.3. 1	Site Organization.....	A-23.
3.3. 2	Entry Sequence.....	A-24.
3.3. 3	Standards.....	A-24.
3.3. 4	Grading Standards.....	A-24.
3.3. 5	Landscape Guidelines.....	A-25.
3.3. 6	Parking.....	A-26.
3.3. 7	Access Drives.....	A-27.
3.3. 8	Loading and Special Equipment Areas.....	A-27.
3.3. 9	Exterior Walls and Fences.....	A-28.
3.3.10	Signage and Entry Monumentation.....	A-29.
3.3.11	Architectural Guidelines.....	A-29.
3.4	Business Park.....	A-33.
3.4. 1	Site Organization.....	A-33.
3.4. 2	Entry Sequence.....	A-33.
3.4. 3	Standards.....	A-33.
3.4. 4	Grading Standards.....	A-34.
3.4. 5	Landscape Guidelines.....	A-34.
3.4. 6	Parking.....	A-35.
3.4. 7	Access Drives.....	A-37.
3.4. 8	Loading and Special Equipment Areas.....	A-37.
3.4. 9	Exterior Walls and Fences.....	A-38.
3.4.10	Signage and Entry Monumentation.....	A-38.
3.4.11	Architectural Guidelines.....	A-39.
3.5	Commercial Center.....	A-42.
3.5. 1	Site Organization.....	A-42.
3.5. 2	Entry Sequence.....	A-43.
3.5. 3	Standards.....	A-43.
3.5. 4	Grading Standards.....	A-43.
3.5. 5	Landscape Guidelines.....	A-43.
3.5. 6	Parking.....	A-44.
3.5. 7	Loading and Special Equipment Areas.....	A-45.
3.5. 8	Exterior Walls and Fences.....	A-46.
3.5. 9	Signage and Entry Monumentation.....	A-46.
3.5.10	Architectural Guidelines.....	A-47.

SECTION 4.	PERFORMANCE STANDARDS.....	A-49.
4. 1	Landscape Guidelines.....	A-50.
4.1.1	General Guidelines.....	A-50.
4.1.2	Plant Selection.....	A-51.
4.1.3	General Plant List.....	A-51.
4.1.4	Recommendations.....	A-52.
4.1.5	PG&E Easement Planting.....	A-53.
4.1.6	Fire Management.....	A-54.
4. 2	Grading Guidelines.....	A-55.
4.2.1	General Guidelines.....	A-55.
4.2.2	Guidelines for Maximum Gradients.....	A-57.
4. 3	Construction Standards.....	A-57.
4.3.1	Roads.....	A-57.
4.3.2	Bikeway System.....	A-57.
4.3.3	Pedestrian Circulation.....	A-58.
4.3.4	Transit Stops.....	A-58.
4.3.5	Construction.....	A-58.
4.3.6	Flood Damage Protection Ordinance.....	A-58.
4.3.7	Water and Wastewater.....	A-58.
4.3.8	Utilities.....	A-59.
4. 4	Erosion Control.....	A-59.
4. 5	Noise.....	A-59.
4. 6	Solid Waste Management.....	A-60.
4. 7	Glare.....	A-60.
4. 8	Odor.....	A-60.
4. 9	Air Quality.....	A-60.
4.10	Transportation Systems Management (TSM) Program Standards.....	A-60.
4.10.1	Goals and Measures.....	A-61.
4.10.2	City Commitments.....	A-61.

SECTION 1. INTRODUCTION

The development standards have been established to provide a continuity and quality of design throughout the Northgate Project. Included in the standards are clearly defined design requirements such as street widths, landscape zones, and building setbacks. Qualitative statements support the standards by describing the design intent and purpose.

The development standards have been broken down into three sections. The first section addresses the public improvements for the Northgate development, including public ROW (street standards) and Blue Rock Springs Creek. Although initial improvements will be carried out by the primary developer, continued maintenance shall become the responsibility of the appropriate public agency.

The second section addresses private improvements which are the responsibility of the parcels' developers. Ongoing maintenance shall be the responsibility of the owners/tenants.

The third section contains performance standards. Grading guidelines, construction standards, fire management, erosion control, noise control and air quality are all addressed in this section.

All development within Northgate shall conform to the following development standards and referenced standards in the Vallejo Municipal Code.

SECTION 2. PUBLIC IMPROVEMENT GUIDELINES

2.1 SCOPE OF THESE GUIDELINES

The overall visual continuity of the development will rely on the use of plant material that is native and indigenous to the hillsides of the Bay Area and is drought tolerant. The concept is to create a semi-informal "woodland" setting using linear plantings of street trees, backed by groves of trees, and an understory of shrubs and ground covers along all major public ROWs. The roadways are to be intensely planted, both the medians and the remaining ROW and easement to soften architectural edges and reduce the scale of the streets, creating a park-like image.

Open lands within fifty feet of the street curb shall be designed and maintained in accordance with the Fire Department standards and policy, keeping in mind the design intent of the project as a whole. The fire break zone may be under private ownership; however, the land will be maintained under a Landscape Maintenance Assessment District.

Rindler Creek will be relocated and improved in a manner consistent with the design theme. Only native plant material will be used in the restorative planting.

All public improvements shall be constructed to the City of Vallejo's and Vallejo Sanitation and Flood Control District's standards, and are subject to the City's and VSFCD's approval.

Defensible space and traffic safety design techniques shall be used in all aspects of the project.

2.2 ROAD STANDARDS

Roadway cross sections have been developed throughout the Northgate area based on the future projections of traffic. Figure IV-1 in the Supplement to the FEIR summarizes the roadway requirements at intersections and identifies future signalized intersections. The following sections describe each roadway and their typical mid-block cross section.

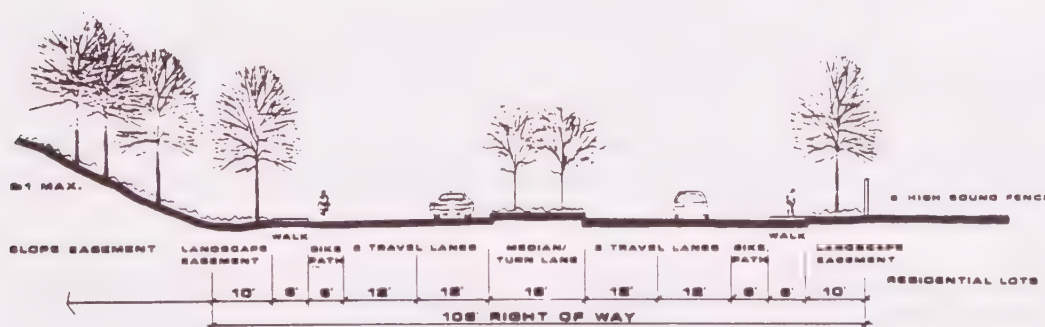
Noise mitigation measures shall be used on Columbus, Ascot, Turner and Redwood Parkways when traffic noise levels exceed City of Vallejo standards.

2.2.1 Columbus Parkway

Columbus Parkway is a primary truck route and major connector to the I-80 corridor. The character of the major arterial retains a scenic and informal quality. The improvement standards for Columbus Parkway shall be compatible with the City's design standards.

2.2.2 Ascot Parkway

Ascot Parkway continues the park-like character, accented by rows of trees and groves of trees. Its primary function is to connect areas within Northgate Specific Area Plan to Columbus Parkway at the north and south. This road is an important access street for the north-south flow of traffic toward the commercial and office areas. Only truck traffic with the Business Park as a destination will be allowed on Ascot; signage will discourage through truck traffic into the residential neighborhoods. No trucks shall be allowed on Ascot beyond the Ascot Parkway/Turner Parkway intersection. Access will be controlled with limited right-in/right-out connections at locations other than major streets.



Typical Section - Ascot Parkway

Ascot Parkway Standards:

- a. 108-foot ROW;
- b. Four vehicle lanes;
- c. 16-foot median, or 4-foot median with 12-foot turning lane;
- d. Six-foot bicycle lanes on both sides;
- e. No parking on the roadway;
- f. Signage as required;
- g. Lighting to meet City of Vallejo Public Improvement Standards;
- h. All utilities shall be placed underground.

Landscape Zone containing:

- a. Six-foot walk;
- b. 25-foot minimum parking lot setback from face of curb.
- c. Where Ascot Parkway abuts open space, a 25-foot wide landscape zone and 25-foot disced fire break adjacent to the ROW shall be required.

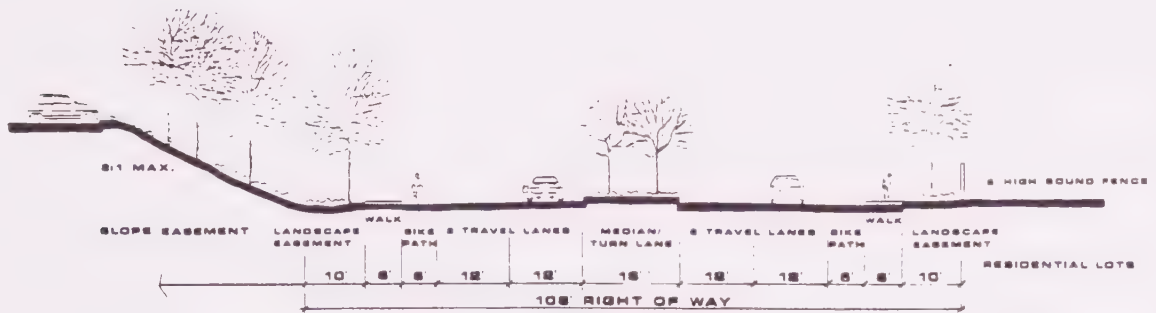
Landscaping:

- a. All landscape and irrigation plans shall be reviewed by the City of Vallejo for approval;
- b. Street trees on both sides at minimum of 40-foot spacing;
- c. Informally grouped groves (three or more) of medium to tall trees
- d. Trees in landscape zone, 15-gallon minimum; groves to be spaced 40 feet apart maximum.
- e. Small accent trees planted in median, 15-gallon minimum with ground cover, except riverstone in areas 2-foot wide or less.
- f. Low hedge planting or berming to screen parking lots.

2.2.3 Turner Parkway

Turner Parkway is a connecting street between Admiral Callaghan Lane and Ascot Parkway. The realignment of Turner will eliminate the impact of the street on the existing Hunter Ranch

neighborhood. Turner Parkway will serve as a collector road for local access from residential areas to commercial areas of the Northgate SAP. Presently the street provides access to the Hunter Ranch development. The revised and realigned Turner Parkway would retain limited access to the Hunter Ranch development minimizing any through traffic. Turner Parkway is proposed as a controlled access street with median breaks at four locations. No truck traffic will be allowed on Turner Parkway except for local deliveries.



Typical Section - Turner Parkway

Turner Parkway Standards:

- a. 108-foot ROW;
- b. Four vehicle lanes;
- c. 16-foot median, or 4-foot median with 12-foot turning lane;
- d. Six-foot bicycle lanes on both sides;
- e. No parking on the roadway;
- f. Signage as required;
- g. Lighting to meet City of Vallejo Public Improvement Standards;
- h. All utilities shall be placed underground.

Landscape Zone containing:

- a. Informally grouped groves (three or more) of tall and medium trees, 15-gallon minimum with groves to be spaced 40 feet apart maximum.
- b. Street trees on 40-foot minimum spacing.

Landscaping:

- a. All landscape plans shall be reviewed by the City of Vallejo;
- b. A formal street tree program will be established (40 feet apart maximum);
- c. Ground cover and shrubs throughout;
- d. Small accent trees, 15 gallon minimum, in medians with ground cover, except riverstone in areas four feet or less;
- e. Shrub planting to screen parking lots;
- f. Embankment to be densely planted for erosion control and screening.

2.2.4 Redwood Parkway

Redwood Parkway is an extension of the existing Redwood Parkway and the landscaped parkway theme. Redwood Parkway is the primary access road to the low density residential neighborhoods. Redwood Parkway provides freeway access for the residential developments in the Hunter Ranch and Northgate areas.



Typical Section - Redwood Parkway

Redwood Parkway Standards:

- a. 108-foot ROW;
- b. Four vehicle lanes;
- c. 16-foot median, or 4-foot median with 12-foot turning lane;
- d. No parking on roadway;
- e. Signage as required;

- f. Lighting to meet City of Vallejo Public Improvement Standards;
- g. All utilities shall be placed underground;
- h. 6-foot walk on both sides.

Landscape Zone containing:

- a. Irrigated, 35-foot minimum both sides;
- b. 13-foot landscape easement outside of ROW;
- c. 50-foot fire break (25-foot irrigated landscape zone plus a 25-foot disced zone where Redwood Parkway abuts open space).
- d. Informally grouped groves (three or more) of tall and medium trees, 15-gallon minimum with groves to be spaced 40 feet apart maximum;
- e. Street trees on 40-foot minimum spacing.

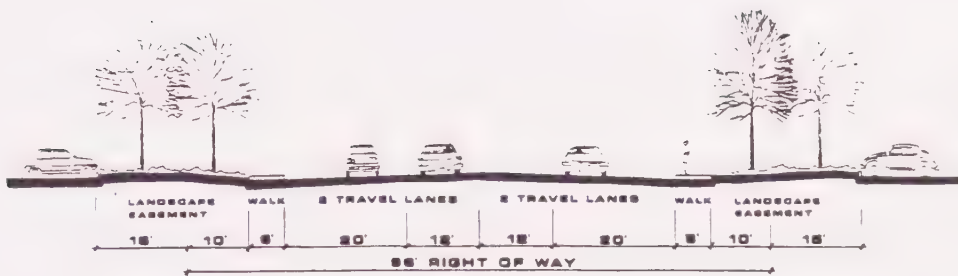
Landscaping:

- a. All landscape plans shall be reviewed by the City of Vallejo.
- b. A formal street tree program will be established (40 feet apart maximum).
- c. Ground cover and shrubs throughout;
- d. Small accent trees, 15 gallon minimum, in medians with ground cover, except riverstone in areas four feet or less;
- e. Tall shrubs planted to break up expanse of wood fences at rear of landscape zone or landscape easement.

2.2.5 Plaza Drive

Plaza Drive will be extended from its current point of construction and will stop at Turner Parkway. This street will provide the primary access to both the commercial and office developments. Plaza Drive will function as a distribution roadway to adjacent development, and will provide collector access to residential areas south of Turner Parkway. Unlike the other streets within the development area, this street will not be a parkway. Appropriate turning lanes will be provided as required. The roadway will be designed to be flexible such that a four or five lane cross section could be striped.

The street standard for Plaza Drive shall depend on the development on either side of the street and the points of access to serve such a development.



Typical Section - Plaza Drive

Plaza Drive Standards:

- a. 96-foot median including median;
- b. Two 12-foot and two 20-foot vehicle lanes (four total lanes or restriped to five if required);
- c. No parking on roadway;
- d. Signage as required;
- e. Lighting to meet City of Vallejo Public Improvements Standards;
- f. 25-foot irrigated landscape zone behind walks;
- g. 6-foot walkway on both sides.

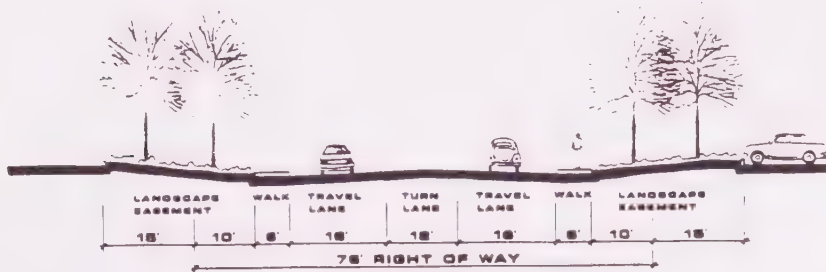
Landscaping:

- a. All landscape plans shall be reviewed by the City of Vallejo;
- b. A formal street tree program will be established (40 feet apart maximum);
- c. Informally grouped groves (three or more) of medium and tall trees in landscape zone with groves to be spaced 40 feet apart maximum;
- d. Trees: 15 gallon minimum;
- e. Small accent trees in median 15 gallon median with ground cover except riverstone in areas two feet wide or less;
- f. Shrub planting to screen parking;

- g. Ground cover and shrubs throughout;
- h. 15-foot landscape easement adjacent to ROW.

2.2.6 Business Park Collector Road

Within the Business Park a proposed loop street will serve the various parcels. The collector will access Columbus Parkway at two locations. The roadway will have a center left turn lane and one lane in each direction.



Typical Section - Business Park Collector

Business Park Collector Standards:

- a. 78-foot ROW;
- b. Two vehicle travel lanes, one center turn lane;
- c. Lighting to meet City of Vallejo Public Improvement Standards;
- d. No parking allowed on roadway;
- e. Signage as required;
- f. All utilities shall be placed underground;
- g. 25-foot irrigated landscape zone containing six-foot walk on both sides and 10-foot landscape easement on both sides.

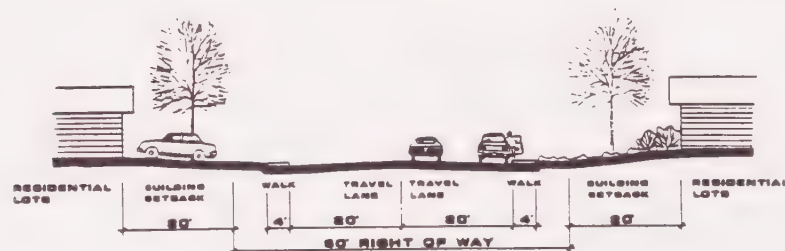
Landscaping:

- a. All landscape plans shall be reviewed by the City of Vallejo;
- b. A formal street tree program will be established (40 feet apart maximum);

- c. Informally grouped groves (three or more) of tall trees in landscape zone, 15 gallon minimum with groves to be spaced 40 feet apart maximum;
- d. Ground cover and shrubs throughout areas except riverstone in medians two feet wide or less;
- e. Small accent trees, 15-gallon minimum, planted in medians;
- f. Shrub planting to screen parking;
- g. 10-foot landscape easement within ROW and 15-foot landscape zone outside and adjacent to ROW.

2.2.7 Residential Collector Streets

Residential collector streets will provide access to fronting houses. City of Vallejo standards are used. These streets function to collect traffic in neighborhoods and connect to arterials.



Typical Section - Residential Collector

Residential Collector Standards:

- a. 60-foot ROW;
- b. Two vehicle lanes (40 feet curb to curb);
- c. 4-1/2-foot walk both sides (4-foot walk, 6-inch curb);
- d. Parking both sides of the roadway;
- e. Signage as required;
- f. Street lighting to meet City of Vallejo Public Improvement Standards;
- g. All utilities in the residential are shall be placed underground.

Landscaping:

- a. Street trees (designated for each street) to be planted 3-10 feet behind the walk with a minimum of one tree per lot or every 50 feet, whichever is less; three trees per corner lot.

Old Turner Parkway:

Due to the realignment of Turner Parkway, the old Turner will be renamed, signed "NOT A THROUGH STREET", and rebuilt to the following specifications:

Between "A" and "B" Streets:

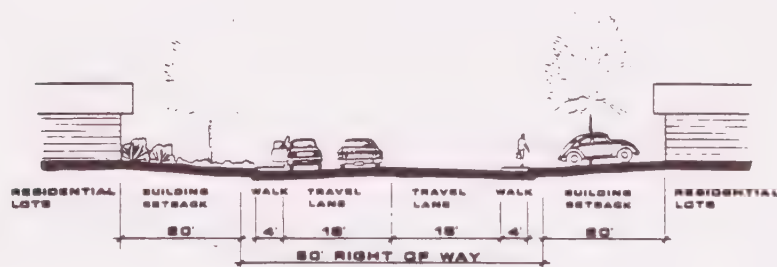
- a. 50-foot ROW;
- b. 36-foot street curb to curb;
- c. 4-foot sidewalks on both sides.

Between "A" Street and Ascot Parkway:

- a. 50-foot ROW;
- b. 40-foot street curb to curb;
- c. 4-foot sidewalks on both sides.

2.2.8 Residential Streets

Residential streets provide access to fronting houses. City of Vallejo standards are used. These streets are local in nature and connect to collector streets.



Typical Section - Residential Street

Residential Street Standards:

- a. 50-foot ROW;
- b. 32-foot curb to curb;
- c. 4-1/2-foot walk on both sides (4-foot walk and 6-inch curb);
- d. Parking on both sides of the roadway;
- e. Street lighting to meet City of Vallejo Public Improvement Standards.
- f. Signage as required;
- g. 6-foot public utilities and planting easement on both sides of ROW.

Landscaping:

- a. Street trees (designated for each street) to be planted 3-10 feet behind the walk, with a minimum of one street tree per lot or every 50 feet, whichever is less; three trees per corner lot.

2.2.9 Residential Cul-de-sacs Serving 20 Homes or Less

Residential cul-de-sac street are short roadways which serve 20 homes or less. These streets are not continuous and are intended to serve fronting homes. City of Vallejo standards are used.



Typical Section - Residential Cul-de-sac

Residential Cul-de-sac Standards:

- a. 40-foot ROW;
- b. 32-foot street curb to curb;

- c. 40-foot radius of cul-de-sac;
- d. 4-foot walk (3-1/2-foot walk and 6-inch curb);
- e. Signage as required;
- f. Street lighting to meet City of Vallejo Public Improvement Standards;
- g. 6-foot public utilities and planting easement on both sides of ROW.

Landscaping:

- a. Street trees (designated for each street) to be planted 3-10 feet behind the walk, with a minimum of one tree per lot or every 50 feet, whichever is less; three trees per corner lot.

2.2.10 Transit (Bus) Stops

A bus turnout is bus stop/zone located in a recessed curb area on the street (separate from moving lanes of traffic).

General Placement Considerations:

Bus turnouts should be constructed at locations wherever the potential for auto/bus conflicts is high. Special attention should be given to roadways with high traffic speeds or traffic volumes where a bus stopping in a travel lane may be unsafe. Turnouts, like bus stops, are preferred at the far side of an intersection whenever possible.

Residential Area:

All arterials and collectors shall accommodate bus service. The layout of the street system and the location of bus stops should be such that all homes are within 1/4-mile of a bus stop.

Provide adequate width for bus pullouts on arterial streets. Also provide adequate space for benches and bus shelters.

Grades on arterial streets should not exceed 10 percent. Maximum grade on all other streets should be 12 percent.

Provide sidewalks for pedestrian access to bus stops.

Business/Office Park:

Employees should be within 1/4-mile of a bus stop, but preferably within 800-900 feet.

Install sidewalks as necessary.

Buildings should be placed near the street with parking at the rear of the site to provide more convenient pedestrian access to the sidewalk and bus stops.

2.2.11 Landscape Maintenance Assessment District

A landscape maintenance assessment district may be established to care for all landscaped areas within and along the public ROW of the streets. Establishment of this or any other maintenance zone will be under the review and control of the City of Vallejo. This includes:

- . Medians and planted areas between street curb and private properties;
- . Fire breaks abutting all public streets.

All disturbed areas within the public ROW shall be landscaped in accordance with the landscape guidelines. All landscape and irrigation plans shall be approved by the City of Vallejo.

The developer will provide level maintenance access to either the top or toe of all large, isolated banks (15 feet or more) at the time of the preliminary grading.

2.3 OPEN SPACE

2.3.1 Trails

Pedestrian access will be limited throughout the residential open space. The limited trailways will serve a dual purpose of providing access to PG&E power lines and access for the maintenance of the fire breaks. Access for the PG&E easement will be designed to PG&E standards.

2.3.2 Erosion Control

Erosion control standards shall be enforced on all disturbed land (see "Performance Standards").

All cut and fill slopes steeper than 5:1, regardless of height, shall be planted for erosion control. Interceptor drains and erosion control netting shall be installed as required.

2.3.3 Fire Management

All 25-foot wide fire break zones will be maintained adjacent to all residential areas and streets. It is recommended to plant ground cover for optimum fire control near residential areas; all other buffers can be disc'd.

2.3.4 Wildlife Management

The 421.8 acre hillside site shall be maintained as a visual amenity and reserve to promote and protect wildlife.

All disturbed land shall be hydroseeded with a California native hydroseed mix.

No development except for public facilities shall occur north of Columbus Parkway east of the PG&E powerline easement which parallels Ascot Parkway.

Pedestrian access shall only be provided on the south side of Columbus Parkway to discourage use of the hillside.

The property will be fenced and posted to prevent trespassing.

2.3.5 Rindler Creek

Rindler Creek will continue the park-like image established along the streets. The creek will be relocated to the north of Columbus Parkway to be adjacent to the open space, and will include pedestrian and bicycle access between the creek and the parkway. Heavily planted with native vegetation, the creek will provide wildlife habitat.

One hundred-foot minimum creek easement.

Twenty four-foot minimum low flow channel with 2:1 slopes, lined with rock or gabion structures; concrete channel is not acceptable.

Forty eight-foot minimum for high flow channel with 3:1 slopes, grass and reed plantings.

Twenty-foot minimum setback from top of bank or roadbed.

The relocation design for Rindler Creek shall be done in accordance with the Revegetation Manual published by Alameda Flood Control District, 1983, and to the standards of the Department of Fish and Game, the Army Corps of Engineers, and the Vallejo Sanitation and Flood Control District.

A naturalized creek bed with native riparian vegetation shall be incorporated in the design of the relocation of Rindler Creek. Flood control and maintenance will also be considered in the Creek's realignment.

All landscape materials within this zone shall be based on lists in the Revegetation Manual developed by Alameda County Flood Control District.

The creek bed shall be designed to be meandering and have variable width as is allowable.

Once the creek has been relocated and revegetated to the standards of the Corps, DFG and VSFCD, VSFCD shall assume responsibility for maintenance.

2.3.6 Blue Rock Springs Corridor

Blue Rock Springs Corridor is the southern boundary of the site. A master plan has been completed for the Corridor, establishing clear goals and guidelines, including a 100-foot easement adjacent to the creek. All new housing developments adjoining the corridor should provide easy access to the greenbelt. Strict erosion control will be enforced during the grading and construction of the site to protect the creek from sedimentation. The 100-foot easement where

the creek traverses the site shall be deeded to GVRD to become part of the Blue Rock Springs Corridor.

2.3.7 Wetland Habitat

Within the project area, there shall be no net loss of wetland habitat as a result of the project.

SECTION 3. PRIVATE DEVELOPMENT STANDARDS

The purpose of this section is to provide detailed development standards as part of the Northgate Specific Area Plan. The standards apply to all office, commercial, residential and private recreational and institutional uses, as designated on the Land Use Plan. All such development within Northgate shall conform to the following development standards and referenced standards in the Vallejo Zoning Ordinance. The location of the permitted uses on the property have been as designated on the Land Use Plan.

3.1 LOW DENSITY RESIDENTIAL

Private improvements are the responsibility of the residential developer.

3.1.1 Primary Uses Permitted

Single family detached residences.

3.1.2 Purpose and Intent

The low density residential provides for the development of detached single family residences at a density not to exceed approximately 1,524 units on 312.5 acres.

3.1.3 Auxiliary Uses Permitted

Public Uses:

Parks, recreational facilities, religious assembly, residential or child care facilities, essential services (as defined in the Vallejo Zoning Ordinance).

Private Uses:

The parking or storage of trailers, recreational vehicles, boats or other special purpose, limited use vehicles shall only be permitted in residential areas when such vehicles are screened from the public view.

Second family residential and home occupations as defined and regulated in the Vallejo Zoning Ordinance.

Residential units shall not be permitted to be used for commercial purposes, except as provided for in Chapter 16.60, Home Occupations, of the Vallejo Municipal Code.

3.1.4 Site Development Standards

Individual Lots:

Each dwelling unit shall be located on an individual lot of record and there shall not be more than one dwelling unit on any one lot.

Lot Area:

Each lot shall contain a minimum of 3,600 square feet of flat useable area not exceeding 10% slope. Minimum total lot size is 4,500 square feet.

Maximum Coverage:

No more than 50% of any lot shall be occupied by a residential and accessory structure.

Lot Width:

Each lot shall be a minimum of 45 feet in width at the building setback line. Corner lots shall have a minimum width of 50 feet.

Lot Depth:

Each lot shall be a minimum of 80 feet in depth of flat, useable area not exceeding 10% slope. Average lot depth shall be 100 feet.

Building Setbacks:

Each building or structure shall be set back the following minimum distances:

Corner Lots:

Setbacks from the streets (non-garage side) shall be subject to review by the Planning Division. Setbacks from the property line to the garage door will be a minimum of 20 feet. The rear yard shall have a minimum of 20 feet as measured from the rear of flat useable area not exceeding 10% slope. Minimum distances between structures will be 10 feet or subject to review by the Planning Division during submission of subdivision and Unit Plan applications.

Interior Lots and Through Lots:

Setbacks from the property line to the garage door will be a minimum of 20 feet. Minimum distances between structures will be 10 feet or subject to review by the Planning Division during the submission of subdivision and unit plan

applications. The rear yard shall have a minimum depth of 20 feet, as measured from the rear portion of the house, of flat useable area not exceeding 10% slope.

Maximum Height:

35 feet.

Accessory Structures:

Private garages and other detached structures shall conform to the standards of the Vallejo Zoning Ordinance. carports shall not be permitted. Garage conversions shall not be permitted.

Satellite dishes shall only be permitted in rear yards when totally screened from the street and other public areas.

Golf Course:

Those residential lots that are adjacent to the golf course shall include landscaping, fencing or other methods deemed appropriate by the City to reduce the impacts of golf balls on residents. In addition, the CC&Rs shall include language that will indemnify the City and the Vallejo Golf Club from residential property damage from golf balls.

Required Parking:

Two enclosed, off-street parking spaces shall be provided for each dwelling unit totally within the limits of the designated lot.

Fencing:

Along the rear lot line for view lots, either a six-foot high wood frame wire fence or maximum four-foot high solid fence shall be permitted. The random mixture of these two fence types shall be avoided. Along the rear lot lines of non-view lots and along side lot lines, a maximum of six-foot high solid wood fence shall be permitted. The maximum fence height within the front setback shall be four feet, unless a lower height is needed to preserve sight distance. Chain link fencing will not be permitted along lot lines abutting open space. Barbed wire and razor wire will not be permitted. Fencing along the top of slopes of open space parcels shall be set back three feet from the top of slope to accommodate potential erosion of the slope.

Utilities:

Underground facilities shall be required for all utilities.

Subdivision Entries:

Subdivision identification signage may be provided at the access road into each of the eight low density residential neighborhoods. This signage shall not exceed 24 square feet

per neighborhood and shall not have interior illumination. Additionally, entries will be defined by one or more of the following: crosswalks with accent plantings, entry walls, entry gates, sculptures and special plantings. Such signage shall not block the line of sight of traffic entering and leaving the neighborhoods.

Recreation:

Recreational and/or neighborhood amenities shall be incorporated where feasible into the low density neighborhoods.

Signage:

Signage shall be in accordance with the Vallejo Sign Ordinance for low density residential district. The project sponsor(s) of all the residential projects shall participate in a unified advertising signage program.

Lighting:

Developers are required to provide lighting for the house address number.

3.1.5 Required Landscaping

All yards abutting a street shall be landscaped with predominantly drought-resistant trees, shrubs and groundcover. A maximum of 25% of the front yard may consist of landscape bark and rock.

A minimum of one 15-gallon street tree per 40 lineal feet of street frontage shall be provided and installed by the developer 3-10 feet behind the sidewalk.

Open space and fire transition zone: All properties located adjacent to large unirrigated open spaces shall be required to maintain a 50-foot zone in accordance with the City of Vallejo's Weed Abatement Ordinance and Fire Department policy. (This includes a 25-foot wide irrigated zone, plus a 25-foot wide zone which is disced, cut or mowed by may 1 of each year.)

3.1.6 Landscape Standards: See Section 4: Performance Standards

3.1.7 Grading Standards

All proposed grades are subject to the review of the Public Works Department. Slopes shall not exceed 2:1.

3.1.8 Architectural Standards

Residential units should be sited to take advantage of passive solar heating techniques, when consistent with the requirements of Titles 24 and 25. When optimum siting is not possible, residential units should be designed with overhangs on south and west elevations.

Within each project subdivision, compatible variations in building height, front elevations, materials and colors shall be sought to achieve an interesting streetscape. Rear and side

elevations shall also be properly treated for an attractive appearance, and shall be consistent with front elevations.

Variations in design, colors and materials are encouraged between different subdivisions to create distinctive sub-neighborhoods within the overall residential area.

Materials should be sensitive to blending with surroundings: wood, stucco painted in soft tones, and/or brick for siding materials. Recommended roof materials include tile, metal or other flame retardant materials. Wood shingle roof are discouraged because of potential fire hazard.

The dominance of garage doors along the streetscape should be avoided by providing a deeper setback for garage doors in relationship to the front of the house, including models with garage doors faced 90 degrees to the street or splitting the garage facade to reduce the impact of the single garage door.

In areas A and G, the architectural styles of the units shall be compatible with those of the adjacent Hunter Ranch neighborhood.

3.2 MEDIUM DENSITY RESIDENTIAL

Private developments are the responsibility of the residential developer.

3.2.1 Primary Uses Permitted

Multi-family residences including single family attached, townhouses, condominiums and apartments.

3.2.2 Purpose and Intent

The medium density residential category provides for a variety of multiple family residences including attached single family residences, townhouses, condominiums and apartments at a density not to exceed 16.4 units per gross area.

3.2.3 Auxiliary Uses Permitted

Public Uses:

Parks, recreational facilities, landscaping, religious assembly, residential or child care facilities and essential services, as defined in the Vallejo Zoning Ordinance.

Residential or Private Uses:

Home occupations as defined in the Vallejo Zoning Ordinance. Garages and carports for storage of automobiles. Outdoor storage of boats, trailers and recreational vehicles is prohibited.

3.2.4 Site Development Standards

Attached single family units and multi-family units including townhouses, condominiums, and apartments shall comply with the following standards:

Building Site:

For the purpose of this section, a "building site" shall mean any lot, group of lots, or building structure containing one or more dwelling units.

Density:

16.4 units per acre.

Orientation:

Solar exposure and views are to be considered in siting buildings. Buildings should be clustered and oriented around common open spaces rather than parking facilities.

Building Setbacks:

For townhouses, condominiums and attached single family units, the following standards shall apply:

Minimum building setback from a public ROW:

Thirty feet from an arterial or collector; 20 feet from other streets.

Minimum distance between any building and exterior property line:

Twenty feet.

Minimum distance between any building located on the site or group of sites:

Twenty five feet, plus two feet for each additional foot over 24 feet.

Minimum distance between any building and parking lot:

Fifteen feet; 25 feet for two story.

Parking Lot Setback:

Parking lots shall be setback 21 feet minimum from public ROWs of Ascot Parkway and Turner Parkway and 10 feet from all other streets. See Ascot Parkway/Hunter Ranch section for typical condition.

Minimum distance between parking lot and exterior parking line not abutting Ascot Parkway and Turner Parkway is 10 feet.

Recreation Facilities:

Swimming pools, club house, recreation center or tot lots, are encouraged to be provided for adults and children. Sufficiency of such facilities will be determined by the City during Unit Plan review.

Open Space:

Minimum of 300 square feet useable open space per unit, 15-foot minimum width at ground level and 8-foot minimum width in deck space per unit. Required setback area shall not be included in the required open space.

Building Height:

No building or structure shall exceed a height of 35 feet.

Required Parking:

Parking shall be provided in accordance with the City of Vallejo Zoning Ordinance.

Signage:

Signage shall be in accordance with the Vallejo Sign Ordinance for medium density residential units. The project sponsor(s) of all the residential projects shall participate in a unified advertising signage program.

Fencing:

Where multi-family housing abuts a single family area, a 6-foot high solid fence or wall shall be erected along the property line. Where such housing abuts open space, a 6-foot high solid fence or view fence shall be installed along the property line. Appropriate fencing shall be determined at the Unit Plan review stage.

Circulation:

Separate pedestrian access should be provided in the interior of a development. Walks shall be a minimum of 4 feet wide. Pedestrian access will be provided to parks and other recreational facilities. Access points shall be determined during site design of the Unit Plan review stage (tentative map).

3.2.5 Grading

Cluster housing with variations and undulations in topography should be encouraged. On slopes greater than 20%, structures should be stepped into the hillside with parking placed below the dwelling units to reduce the amount of grading and paving. High and multiple retaining walls should be discouraged.

The maximum gradient for cut and fill slopes adjacent to open space shall typically be 3:1. For all other slopes, the maximum shall be 2:1.

3.2.6 Required Landscaping

The landscaping of the medium density residential development is to be part of the community-wide image of native and drought tolerant plants. To this end, a plant list has been provided in Section 4 ("Performance Standards - Landscape Guidelines") from which plant choices will be made. Design criteria are provided for fire management, erosion control and slope stabilization.

All areas to be landscaped including setback areas from public ROWs, areas between buildings, parking lots, and exterior property lines shall be landscaped with material selected from the Plant Selection list. In public improvements, all planting is to be irrigated by automatic irrigation systems installed by the developer.

All parking lots shall be screened from the street with low shrubs and/or earth berms. Trees shall be planted in the interior of parking lots at the rate of one tree per six cars, where there is no overhead structure.

Ends of all parking rows shall include landscape medians.

All trash enclosures shall be screened by solid wood fencing or walls, and landscaped with appropriate plantings. Trash enclosures shall be located away from public view and pedestrian areas.

Transition zone: All properties located adjacent to large unirrigated open spaces shall be required to maintain a 50-foot zone in accordance with the City of Vallejo's Weed Abatement Ordinance and Fire Department policy. (This includes a 25-foot wide irrigated zone, plus a 25-foot wide zone which is disced, cut or mowed by May 1 of each year.)

All embankments between the developed site and the street shall be landscaped and irrigated.

Private improvement street-side landscaping shall follow guidelines for the particular street for grading and planting of ground cover, shrubs and trees. Shrubs shall screen parking areas. All areas are to be automatically irrigated. Planting requirements along streets include 15-gallon trees minimum to be planted in groves (3 or more). Groves to be planted 40 feet apart maximum.

3.2.7 Architectural Standards

Materials should be sensitive to blending with surroundings: wood, stucco painted in soft earth tones, and/or brick for siding materials. Roofs should be tile, metal or other flame retardant material. Wood shingle roofs are discouraged as a roofing material because of potential fire hazard.

Multi-family housing, when located adjacent to a single family area, shall be compatible in scale, materials and architecture.

Flat, blank wall surfaces should be minimized through the use of wall projections and recesses, balconies and window details.

On steep slopes, buildings should be stepped into the hillside, with consideration given to placing parking underneath the units.

Enclosed garages should be used instead of open carports. Garages and carports shall match the architectural design, colors, and materials on the buildings.

All structures shall be designed according to the Uniform Building Code and California State earthquake regulations as given in the California Administrative Code.

Siting of housing clusters should respond to wind directions for winter protection and summer cooling.

Units should maximize solar exposure to the south.

Windows should be openable and shaded in summer to allow natural ventilation and cooling (deep reveals, awnings, overhangs, trellises, etc.).

High R-value insulation should be utilized in walls and ceilings of units to reduce heat loss and gain.

Other passive systems such as greenhouses and skylights should be utilized where applicable for winter heat gain.

Landscaping, awnings, cross ventilation and insulation should be used for summer cooling.

3.3 OFFICE PARK

3.3.1 Site Organization

Site design standards are presented in order to balance the opportunities for project identity and the need for efficient site use. Established setbacks and standards will provide a unity within the development, while encouraging individual projects.

Buildings should be clustered around "public" spaces such as landscaped areas, pedestrian plazas or water features.

Buildings should be sited to take advantage of views to the surrounding land forms.

Buildings should be sited to reduce the visual impact parking from the street.

Pedestrian connections should be provided to the street to accommodate transit use.

Pedestrian areas should be separated from parking lots and service areas.

Berms shall be used where feasible to screen parking lots from public rights-of-way.

3.3.2 Entry Sequence

The entry to each site should be planned so that traffic is not channeled directly into parking lots. An entry statement and sequence should incorporate signage, landscape corridor and drop-off area to introduce each building to visitors before entering the parking lot.

Secondary access points should be planned for services and emergency purposes and shall be screened from public view.

Landscaped areas shall provide adequate visual access to entrances and exits of all structures.

3.3.3 Standards

Setbacks:

Minimum building setback shall be 50 feet from face of curb.

No buildings shall encroach with the required landscape zones adjacent to the street and property lines.

Minimum building setback from the toe of the open space slope shall be 50 feet.

Minimum building setback from Rindler Creek shall be 50 feet.

FAR and Coverage:

Building coverage shall not exceed 25% of the total gross lot area.

The floor area ratio (FAR) shall not exceed .32.

Building Height:

Maximum building height shall be 90 feet.

Lot Standards:

Minimum lot size or project area shall be one acre. Smaller lot sizes will be allowed as part of an integrated development involving the joint use of a common parking lot.

Minimum lot width is 200 feet. No minimum street frontage is required.

3.3.4 Grading Standards

Stepped development pads are encouraged in order to minimize grading and large man-made slopes.

Buildings and parking lots should be sited and designed to blend into the topography as much as possible.

Abrupt cut and fill at the edge of open space areas should be avoided.

Maximum gradient for cut and fill areas and slopes adjacent to open space shall be 3:1, unless otherwise specified. Exceptions shall be reviewed on an individual basis.

The maximum gradient for slopes between development pads shall be 2:1.

3.3.5 Landscape Guidelines

The landscape concept is to retain a visual continuity between the rolling hillsides and the new development. To meet this goal, public roadways are to have intensely planted medians and landscape zones to soften the architectural edges and reduce the scale of the streets creating a park-like image.

The landscape guidelines in the performance standards shall apply to all private improvement landscaped areas found in the Office Park including along roads, between lots, on all embankments or otherwise disturbed lands.

All property owners/developers shall be required to submit planting and irrigation plans at the time of the Unit Plan approval stage.

Private improvement street-side landscaping shall follow guidelines for the particular street for grading and planting of ground cover, shrubs and trees. Shrubs and berms shall screen parking areas. All landscaped areas are to be automatically irrigated.

Planting requirements along streets are 15-gallon trees minimum to be planted in groves (3 or more). Groves to be spaced 40 feet apart maximum.

A plant material list has been developed for the landscape areas of the entire development to provide continuity. A plant palette for the Office Park shall be based on this list.

Along the public rights-of-way of Columbus, there shall be a minimum 35-foot and 37-foot planted zone, respectively. Along interior streets it is a 25-foot planted zone (see Street Sections). There shall be no parking or building allowed within this zone. Bus stops, driveways and pedestrian walks are allowed.

Side and rear property lines shall have a 10-foot wide minimum landscaped zone. Where side or rear property lines abut streets, there shall be a landscape zone in accordance with the street standards.

All cut and fill slopes steeper than 5:1, regardless of height, shall be planted for erosion control. Interceptor drains and erosion control netting shall be installed as required.

The building perimeter landscaping will provide opportunities for a variety of design approaches, depending on the parcel location and building site. The landscape design objectives are as follows:

- . To incorporate the building into the site and provide continuity among the various office buildings;
- . To provide a comfortable transition into the building and enhance its aesthetics.

Planting areas, which may include pedestrian paved areas, walks, entries, plazas and water features, shall be provided on all sides of the building, except in services areas, as follows:

Building Height	Planting Zone
1-3 stories	25-foot front 20-foot sides 15-foot rear
4 + stories	30-foot front 20-foot sides 15-foot rear

Entries:

Major entries shall be visually marked with specimen trees such as flowering trees, very tall trees or other plant material which will distinguish the entries as significant.

Transition Zone:

All properties located adjacent to large unirrigated open spaces shall be required to maintain a 50-foot zone in accordance with the City of Vallejo's Weed Abatement Ordinance and Fire Department policy. (This includes a 25-foot wide irrigated zone, plus a 25-foot wide zone which is disced, cut or mowed by May 1 of each year.)

3.3.6 Parking

The landscape design objectives for parking lots are as follows:

- . To visually break up large paved areas with landscaping;
- . To provide shade.

A minimum of one 15-gallon tree shall be planted in the interior of the parking area for every six standard or compact spaces.

A landscape planter, minimum 5 feet wide, shall be provided at the ends of all parking rows.

Maximum grades for parking lots shall be 5%. In order to most appropriately address the varied terrain and provide safe and comfortable grades, parking lots may need to be terraced.

Parking locations and layouts shall provide for safe and convenient pedestrian circulation. Landscaped pedestrian links to the building with special paving shall be encouraged.

Standard parking stalls shall be a minimum of 9 feet by 18 feet. A 2-foot reduction in length is permitted when the vehicle front overhangs a pedestrian walk (minimum 4 feet wide) or landscaped area.

Compact parking stalls shall be a minimum of 8 feet by 16 feet. A 2-foot reduction in length is allowed when the vehicle front overhangs a pedestrian walk (minimum 4 feet wide) or landscaped area.

Forty percent of the parking area may be designed for compact cars. The number of required spaces shall comply with Chapter 16.62 of the City of Vallejo Zoning Ordinance.

Parking shall not be allowed along the front curb of the building.

Where applicable, shared parking for adjacent uses (where peak use occurs at different times) will be allowed and encouraged to minimize parking lots. The amount of shared parking will be determined by the City during the Unit Plan review stage.

Access easements to allow the interconnection of parking lots should be considered. Such easements would allow the City to require connections between parking lots at a future date. Location easements shall be determined at the Unit Plan review stage.

Parking for the handicapped shall be provided in accordance with California State law.

Special closed-in parking spaces shall be provided for car and van pools. Bicycle racks shall be provided near the building entrance.

High pressure-sodium light fixtures should be used and maximum parking lot illumination shall not exceed .5fc. Maximum standard height is 20 feet.

Lighting should be designed with attention to aesthetics and site security.

Pedestrian pathways should be illuminated separately to a maximum of 1 fc.

3.3.7 Access Drives

Access drives shall be located a minimum of 150 feet apart from center line to center line.

Access drives shall be consolidated to the maximum extent possible.

Entrances to private property from a public street shall be a driveway approach as per City of Vallejo standards.

3.3.8 Loading and Special Equipment Areas

Service, storage, special equipment, maintenance and loading areas shall be screened from view from adjacent buildings, pedestrian areas, streets and driveways. They shall be screened from public view with planting or architectural elements. All service and loading areas will be positioned so service vehicles will not disrupt traffic flow on or off the site.

Loading areas shall be accommodated entirely on-site. Off-site vehicle loading will not be permitted.

Loading and services areas shall be screened from street frontages. Loading docks and services areas must be located on the interior building side or rear yards and concealed from public view.

When utility equipment and communication devices are located on the grounds, the site shall appear free of all such devices. Exterior utilities such as water, gas, sewage, electrical, and communication lines must be installed underground.

Refuse collections areas are to be visually screened with a solid perimeter wall. Screening wall materials and colors shall be compatible with those of the adjacent buildings. Refuse collection areas are to be located on the interior building side or rear yard.

Service, storage and maintenance areas will be constructed and maintained according to the following criteria:

- . No materials, supplies or equipment, including trucks or other motor vehicles, are to be stored on-site except inside a closed building or behind an architectural screening to prevent visibility from neighboring property and streets.
- . All storage areas shall be screened by walls and fences and will be located on the side or rear portions of buildings.
- . Architectural screening shall be constructed of the same materials and finishes as the adjacent building, and shall be designed and placed to complement the building design.
- . No service, storage, maintenance or loading area may extend into a setback area.

3.3.9 Exterior Walls and Fences

Walls and fences shall be used for screening and to define security areas.

No freestanding walls shall be constructed within the landscape zone, unless it is used for signage and entry monumentation purposes.

Walls used for screening shall not exceed 8 feet.

Walls and fences shall be designed as an integral part of the overall site design. They will be constructed with materials that are complimentary to the style of adjacent buildings and incorporate the same finishes and colors.

No barbed wire or razor wire shall be allowed in the Office Park.

Chain link fencing shall not be allowed along street frontages. Where chain link fencing is proposed at other areas of the site, it shall be of a dark color and planted with shrubs and/or vines. Top and bottom rails shall be installed.

3.3.10 Signage and Entry Monumentation

A comprehensive entrance and tenant signage program will be developed for each parcel during the Unit Plan review stage to help unify and provide identity.

All signage shall be integrated with the design, materials and colors of the building.

Illumination of signs shall be ground mounted or integrated with the structure of the sign. No freestanding lamps shall be allowed.

Walls and signage piers should be integrated with the landscape and grading plan as part of a site feature.

All signage shall comply with the City of Vallejo Sign Ordinance and Standards for PO, Professional Office, and IU, Intensive Use, districts except that freestanding "pole" signs and "roof" signs shall not be permitted and monument signs will be permitted up to 50 square feet.

Developers shall provide lighting for the business address number.

The amount of signage for the commercial, business and office uses shall not be determined by parcels. Rather, the amount of signage shall be determined by the size of the overall commercial, business or office project. This is to avoid oversigning when one project may extend over several parcels.

3.4.11 Architectural Guidelines

Introduction:

The purpose of this section is to identify the principal design elements which will tie all the building types together to create a unified project. The guidelines are meant to encourage high quality architectural design. It is intended that building materials and colors be limited to help achieve an integrated design.

Color:

Buildings should be predominantly neutral in color.

Accent colors are acceptable, provided they are secondary to the overall color scheme of each building and the project as a whole. Secondary is defined as less than 10% of the total exterior wall area excluding glass.

Predominant colors and finishes should be flat.

Facade Treatment:

Buildings should have a strong visual and contemporary identity.

Buildings with highly stylized facade treatments which create historical or other inappropriate themes are discouraged. Also discouraged are reflective glass "boxes".

Spandrel elements may be accented with one or two colors, tones of the same color, or changes in texture.

Acceptable glazing colors are clear, grey tints, low reflective grey, blue or green glass. Excessive use of reflective glass or reflective metal surfaces is discouraged.

Building facades should be accented with details such as reveal joints, balcony railings, accent bands (tile, brick or concrete). Accent banding should be secondary to predominantly neutral colors of the major wall materials. If paint is used, it should be used on a different plane, rather than one color masked off from one another. These secondary facade elements can vary in material, color and detailing among buildings.

Various coursings of masonry units can be used to achieve patterns, shadows, etc..

Both horizontal window expressions and punched windows can be used for the primary themes of buildings. Limited use of curtain walls can be used for specific areas of buildings as a special treatment.

The central entry portions of the buildings should be emphasized through changing elements such as glazing, panel colors, size of accent bands, etc..

No pre-engineered metal buildings will be allowed.

Building Massing, Form and Organization:

Terracing of the building floors is encouraged to help reduce the perceived size of the buildings and bring them to a more comfortable, human scale.

Stepped and offset planes are recommended to add visual interest and reduce scale.

All public commercial and support services which would improve the pedestrian environment shall be located on ground level.

Within each development, a well landscaped, wind protected pedestrian environment should be created. This area would best be located between clustered buildings, separate from automobile traffic. These spaces should be sunny and warm in winter and shaded and cool in summer.

Ground Floor Treatment:

The accenting of building bases should be done with attention to detailing, materials, colors, finishes, lighting and/or arcades.

Large, blank walls at building bases should be avoided.

Plaza areas adjacent to buildings should be provided for pedestrian activities. Plaza areas should be protected from the wind and solar orientation should be considered.

Desirable materials for primary pedestrian walls include precast pavers, stone, brick, and poured in place concrete with exposed aggregate.

Building entrances should be made clearly identifiable by use of scale and material changes.

Roof:

Roofs should be flat, built-up types. The only exceptions may be retail or commercial, or child care facilities.

All rooftop equipment shall be organized into major groups. Random placement of rooftop equipment shall not be allowed.

Screens and/or walls shall be used to block ground-level views within the project of all major roof equipment. The integration of mechanical equipment shall be achieved by the following methods:

- . Making the penthouse part of the building mass;
- . Using similar skin materials or colors as on building exterior;
- . Designing penthouse or equipment screens with similar forms as the building.

Exterior components of plumbing, processing, heating, cooling and ventilation systems shall not protrude above the top of any parapet.

Views of exterior components of plumbing, etc., and their screening devices, from the upper floors of adjacent buildings should be minimized by installing those elements in a compact and orderly fashion, and coloring them to blend with their backgrounds.

No exterior components of plumbing, etc., should be mounted on any building walls unless they are an integrated architectural design feature.

Recommended Design Features:

Ceremonial entrance drive.

Well landscaped visitor parking areas.

Highlighted entrance and entry plazas.

Decorative pedestrian plazas and walkways.

Focal site sculptures.

Well landscaped outdoor employee lunch areas.

Atriums and interior courts.

Accent landscaping and water features.

Dynamic building forms.

Light and shadow patterns.

Terracing of buildings.

Recommended Materials:

Brick

Tile with dull finish (wall and paving)

Stone (granite, marble, limestone)

Concrete

Glass (clear, tinted, low-reflective)

Exterior plaster (uniform finish, no textured patterns)

Metal panel

Wood

Energy Considerations:

Buildings should maximize southern exposure, minimize western exposure for extreme heat gain.

Especially low-rise garden office should be cooled through natural ventilation and dense landscaping.

Operable windows should be utilized where possible.

Awnings and other shading devices should be utilized to reduce heat gain, including heat-resistant glass.

All mechanical systems should be energy efficient, minimizing cooling and heating, and utilizing passive systems as much as possible.

Lighting systems should include track lighting to reduce overall loads, and natural lighting should be maximized to reduce lighting loads.

Building materials used should be selected with considerations as to their capabilities in assisting to produce evenness in solar radiation conditions. Where possible, employ medium range colors on sun exposed surfaces. Dark colors should only be used in places protected from summer sun.

Solar panels should be encouraged yet located so that they least impair the visual qualities of the area.

3.4 BUSINESS PARK

3.4.1 Site Organization

Site design standards are presented in order to balance the opportunities for project identity and the need for efficient site use. Established setbacks and standards will provide a unity within the development, while encouraging individual projects.

Buildings should be sited to reduce the visual impact parking from the street.

Pedestrian connections should be provided to the street to accommodate transit use.

Pedestrian areas should be separated from parking lots and service areas.

Berms shall be used where feasible to screen parking lots from public rights-of-way.

3.4.2 Entry Sequence

The entry to each site should be planned so that traffic is not channeled directly into parking lots. An entry statement and sequence should incorporate signage, landscape corridor and drop-off area to introduce each building to visitors before entering the parking lot.

The entry sequence should incorporate signage, landscaped corridor, visitor parking and a drop-off area.

Secondary access points should be planned for services and emergency purposes and shall be screened from public view.

Landscaped areas shall provide adequate visual access to entrances and exits of all structures.

3.4.3 Standards

Setbacks:

Minimum building setback shall be 50 feet from face of curb.

No buildings shall encroach with the required landscape zones adjacent to the street and property lines.

Minimum building setback from the toe of the open space slope shall be 50 feet.

FAR and Coverage:

The floor area ratio (FAR) shall not exceed .32.

Building Height:

Maximum building height shall be 45 feet.

Lot Standards:

Minimum lot size shall be one acre.

3.4.4 Grading Standards

Stepped development pads are encouraged in order to minimize grading and large man-made slopes.

Buildings and parking lots shall be sited and designed to blend into the topography as much as possible.

Abrupt cut and fill at the edge of open space areas shall be avoided.

Maximum gradient for cut and fill areas and slopes adjacent to open space shall be 3:1, unless otherwise specified. Exceptions shall be reviewed on an individual basis. The maximum gradient for slopes between development pads shall be 2:1.

A 30-40-foot elevation change shall separate the business/industrial sites from Turner Parkway (Blue Rock Parkway) east of the PG&E powerline easement (Plaza Drive). Maximum slope shall be 2:1.

3.4.5 Landscape Guidelines

The landscape concept is to retain a visual continuity between the rolling hillsides and the new development. To meet this goal, public roadways are to have intensely planted medians and landscape zones to soften the architectural edges and reduce the scale of the streets creating a park-like image.

The landscape guidelines in the performance standards shall apply to all private improvement landscaped areas found in the Business Park including along roads, between lots, on all embankments or otherwise disturbed lands.

All property owners/developers shall be required to submit planting and irrigation plans at the time of the Unit Plan review stage.

Private improvement street-side landscaping shall follow guidelines for the particular street for grading and planting of ground cover, shrubs and trees. Shrubs and berms shall screen parking areas. All landscaped areas are to be automatically irrigated.

A plant material list has been developed for the landscape areas of the entire development to provide continuity. A plant palette for the Business Park area shall be based on this list.

A 60-foot minimum planted buffer zone shall separate the business/industrial sites from Turner Parkway east of the PG&E powerline easement (Plaza Drive). This buffer shall be maintained by the Business/Industrial Park Association.

Side and rear property lines shall have a 10-foot wide minimum landscaped zone.

All cut and fill slopes steeper than 5:1, regardless of height, shall be planted for erosion control. Interceptor drains and erosion control netting shall be installed as required.

The building perimeter landscaping will provide opportunities for a variety of design approaches, depending on the parcel location and building site. The landscape design objectives are as follows:

- . To incorporate the building into the site and provide continuity among the various office buildings;
- . To provide a comfortable transition into the building and enhance its aesthetics.

Planting areas, which may include pedestrian paved areas, walks, entries, plazas and water features, shall be provided on all sides of the building, except in services areas.

Entries:

Major entries shall be visually marked with specimen trees such as flowering trees, very tall trees or other plant material which will distinguish the entries as special.

Transition Zone:

All properties located adjacent to large unirrigated open spaces shall be required to maintain a 50-foot zone in accordance with the City of Vallejo's Weed Abatement Ordinance and Fire Department policy. (This includes a 25-foot wide irrigated zone, plus a 25-foot wide zone which is disced, cut or mowed by May 1 of each year.)

3.4.6 Parking

The landscape design objectives for parking lots are as follows:

- . To visually break up large paved areas with landscaping;
- . To provide shade.

A minimum of one 15-gallon tree shall be planted in the interior of the parking area for every six standard or compact spaces.

A landscape planter, minimum 5 feet wide, shall be provided at the ends of all parking rows.

Maximum grades for parking lots shall be 5%. In order to most appropriately address the varied terrain and provide safe and comfortable grades, parking lots may need to be terraced.

Parking locations and layouts shall provide for safe and convenient pedestrian circulation. Landscaped pedestrian links to the building with special paving shall be encouraged.

Standard parking stalls shall be a minimum of 9 feet by 18 feet. A 2-foot reduction in length is permitted when the vehicle front overhangs a pedestrian walk (minimum 4 feet wide) or landscaped area.

Compact parking stalls shall be a minimum of 8 feet by 16 feet. A 2-foot reduction in length is allowed when the vehicle front overhangs a pedestrian walk (minimum 4 feet wide) or landscaped area.

Forty percent of the parking area may be designed for compact cars. The number of required spaces shall comply with Chapter 16.62 of the City of Vallejo Zoning Ordinance.

Parking shall not be allowed along the front curb of the building.

Where applicable, shared parking for adjacent uses (where peak use occurs at different times) will be allowed and encouraged to minimize parking lots. The amount of shared parking will be determined by the City during the Unit Plan review stage.

Access easements to allow the interconnection of parking lots should be considered. Such easements would allow the City to require connections between parking lots at a future date. Location easements shall be determined at the Unit Plan review stage.

Parking for the handicapped shall be provided in accordance with California State law.

Special closed-in parking spaces shall be provided for car and van pools. Bicycle racks shall be provided near the building entrance.

High pressure-sodium light fixtures should be used and maximum parking lot illumination shall not exceed .5fc. Maximum standard height is 20 feet.

Lighting should be designed with attention to aesthetics and site security.

Pedestrian pathways should be illuminated separately to a maximum of 1 fc.

Bicycle racks shall be provided in conjunction with the parking area for all nonresidential uses and with the park-and-ride facility.

3.4.7 Access Drives

Access drives shall be located a minimum of 150 feet apart from center line to center line.

Access drives shall be consolidated to the maximum extent possible.

Entrances to private property from a public street shall be a driveway approach as per City of Vallejo standards.

Access drives shall be lined up and adequately marked to minimize traffic hazards.

Local street design in the Business/Industrial Park shall provide adequate turning radii for large trucks.

3.4.8 Loading and Special Equipment Areas

Service, storage, special equipment, maintenance and loading areas shall be screened from view from adjacent buildings, pedestrian areas, streets and driveways. They shall be screened from public view with planting or architectural elements. All services and loading areas will be positioned so service vehicles will not disrupt traffic flow on or off the site.

Loading areas shall be accommodated entirely on-site. Off-site vehicle loading will not be permitted.

Loading and services areas shall be screened from street frontages. Loading docks and services areas must be located on the interior building side or rear yards and concealed from public view.

When utility equipment and communication devices are located on the grounds, the site shall appear free of all such devices. Exterior utilities such as water, gas, sewage, electrical, and communication lines must be installed underground.

Refuse collections areas are to be visually screened with a solid perimeter wall. Screening wall materials and colors shall be compatible with those of the adjacent buildings. Refuse collection areas are to be located on the interior building side or rear yard.

Service, storage and maintenance areas will be constructed and maintained according to the following criteria:

- . No materials, supplies or equipment, including trucks or other motor vehicles, are to be stored on-site except inside a closed building or behind an architectural screening to prevent visibility from neighboring property and streets.
- . All storage areas shall be screened by walls and fences and will be located on the side or rear portions of buildings.
- . Architectural screening shall be constructed of the same materials and finishes as the adjacent building, and shall be designed and placed to complement the building design.

No service, storage, maintenance or loading area may extend into a setback area.

3.4.9 Exterior Walls and Fences

Walls and fences shall be used for screening and to define security areas.

No freestanding walls shall be constructed within the landscape zone, unless it is used for signage and entry monumentation purposes.

Walls used for screening shall not exceed 8 feet.

Walls and fences shall be designed as an integral part of the overall site design. They will be constructed with materials that are complimentary to the style of adjacent buildings and incorporate the same finishes and colors.

No barbed wire or razor wire shall be allowed in the Business Park.

Chain link fencing shall not be allowed along street frontages. Where chain link fencing is proposed at other areas of the site, it shall be of a dark color and planted with shrubs and/or vines. Top and bottom rails shall be installed.

Walls for noise insulation within the Northgate development shall be provided as necessary.

3.4.10 Signage and Entry Monumentation

A comprehensive entrance and tenant signage program will be developed for each parcel during the Unit Plan review stage to help unify and provide identity.

All signage shall be integrated with the design, materials and colors of the building.

Illumination of signs shall be ground mounted or integrated with the structure of the sign. No freestanding lamps shall be allowed.

Walls and signage piers should be integrated with the landscape and grading plan as part of a site feature.

All signage shall comply with the City of Vallejo Sign Ordinance and Standards for PO, Professional Office, and IU, Intensive Use, districts except that freestanding "pole" signs and "roof" signs shall not be permitted and monument signs will be permitted up to 50 square feet.

Developers shall provide lighting for the business address number.

If an auto mall is developed within the Business Park, the City shall consider one off-site sign for the auto mall. The project sponsor shall apply for a major use permit to allow the sign.

The amount of signage for the commercial, business and office uses shall not be determined by parcels. Rather, the amount of signage shall be determined by the size of the overall commercial.

business or office project. This is to avoid oversigning when one project may extend over several parcels.

3.4.11 Architectural Guidelines

Introduction:

The purpose of this section is to identify the principal design elements which will tie all the building types together to create a unified project. The guidelines are meant to encourage high quality architectural design. It is intended that building materials and colors be limited to help achieve an integrated design.

Color:

Buildings should be predominantly neutral in color.

Accent colors are acceptable, provided they are secondary to the overall color scheme of each building and the project as a whole. Secondary is defined as less than 10% of the total exterior wall area excluding glass.

Predominant colors and finishes should be flat.

Facade Treatment:

Buildings should have a strong visual and contemporary identity.

Buildings with highly stylized facade treatments which create historical or other inappropriate themes are discouraged. Also discouraged are reflective glass "boxes".

Spandrel elements may be accented with one or two colors, tones of the same color, or changes in texture.

Acceptable glazing colors are clear, grey tints, low reflective grey, blue or green glass. Excessive use of reflective glass or reflective metal surfaces is discouraged.

Building facades should be accented with details such as reveal joints, balcony railings, accent bands (tile, brick or concrete). Accent banding should be secondary to predominantly neutral colors of the major wall materials. If paint is used, it should be used on a different plane, rather than one color masked off from one another. These secondary facade elements can vary in material, color and detailing among buildings.

Various coursings of masonry units can be used to achieve patterns, shadows, etc..

Both horizontal window expressions and punched windows can be used for the primary themes of buildings. Limited use of curtain walls can be used for specific areas of buildings as a special treatment.

The central entry portions of the buildings should be emphasized through changing elements such as glazing, panel colors, size of accent bands, etc..

Consideration should be made of solar orientation for energy conservation.

Pre-engineered metal buildings must meet the above architectural guidelines.

Building Massing, Form and Organization:

Stepped and offset planes are recommended to add visual interest and reduce scale.

All public commercial and support services which would improve the pedestrian environment shall be located on ground level.

Within each development, a well landscaped, wind protected pedestrian environment should be created. This area would best be located between clustered buildings, separate from automobile traffic. These spaces should be sunny and warm in winter and shaded and cool in summer.

Ground Floor Treatment:

The accenting of building bases should be done with attention to detailing, materials, colors, finishes, lighting and/or arcades.

Arcades are especially encouraged for commercial buildings.

Large, blank walls at building bases should be avoided.

Plaza areas adjacent to buildings should be provided for pedestrian activities. Plaza areas should be protected from the wind and solar orientation should be considered.

Building entrances should be made clearly identifiable by use of scale and material changes.

Roof:

Roof should be flat, built-up types. The only exceptions may be retail or commercial, or child care facilities.

All rooftop equipment shall be organized into major groups. Random placement of rooftop equipment shall not be allowed.

Screens and/or walls shall be used to block ground-level views within the project of all major roof equipment. The integration of mechanical equipment shall be achieved by the following methods:

- . ^ Making the penthouse part of the building mass.

- Using similar skin materials or colors as on building exterior.
- Designing penthouse or equipment screens with similar forms as the building.

Exterior components of plumbing, processing, heating, cooling and ventilation systems shall not protrude above the top of any parapet.

Views of exterior components of plumbing, etc., and their screening devices, from the upper floors of adjacent buildings should be minimized by installing those elements in a compact and orderly fashion, and coloring them to blend with their backgrounds.

No exterior components of plumbing, etc., should be mounted on any building walls unless they are an integrated architectural design feature.

Recommended Design Features:

Ceremonial entrance drive.

Well landscaped visitor parking areas.

Highlighted entrance and entry plazas.

Decorative pedestrian plazas and walkways.

Focal site sculptures.

Well landscaped outdoor employee lunch areas.

Atriums and interior courts.

Accent landscaping and water features.

Dynamic building forms.

Light and shadow patterns.

Terracing of buildings.

Recommended Materials:

Brick

Tile with dull finish (wall and paving)

Stone (granite, marble, limestone)

Concrete

Glass (clear, tinted, low-reflective)

Exterior plaster (uniform finish, no textured patterns)

Metal panel

Wood

Energy Considerations:

Buildings should maximize southern exposure, minimize western exposure for extreme heat gain.

Especially low-rise garden office should be cooled through natural ventilation and dense landscaping.

Operable windows should be utilized where possible.

Awnings and other shading devices should be utilized to reduce heat gain, including heat-resistant glass.

All mechanical systems should be energy efficient, minimizing cooling and heating, and utilizing passive systems as much as possible.

Lighting systems should include track lighting to reduce overall loads, and natural lighting should be maximized to reduce lighting loads.

Building materials used should be selected with considerations as to their capabilities in assisting to produce evenness in solar radiation conditions. Where possible, employ medium range colors on sun exposed surfaces. Dark colors should only be used in places protected from summer sun.

Solar panels should be encouraged yet located so that they least impair the visual qualities of the area.

3.5 COMMERCIAL CENTER

3.5.1 Site Organization

Site design standards are presented in order to balance the opportunities for project identity and the need for efficient site use. Established setbacks and standards will provide a unity within the development, while encouraging individual projects.

Buildings should be sited to reduce the visual impact parking from the street. Buildings should be clustered.

Pedestrian connections should be provided to the street to accommodate transit use.

Pedestrian areas should be separated from service areas.

Berms shall be used where feasible to screen parking lots from public rights-of-way.

3.5.2 Entry Sequence

The entry to each site should be planned so that traffic is not channeled directly into parking lots. An entry statement and sequence should incorporate signage, landscape corridor.

Secondary access points should be planned for services and emergency purposes and as additional access for customers and tradespeople.

Landscaped areas shall provide adequate visual access to entrances and exits of all structures.

3.5.3 Standards

Setbacks:

Minimum building setback shall be 25 feet from face of curb.

No buildings shall encroach with the required landscape zones adjacent to the street and property lines.

FAR and Coverage:

Allowable FAR shall be .25.

Building Height:

Maximum building height shall be 45 feet.

Lot Standards:

No minimum lot size.

3.5.4 Grading Standards

Stepped development pads are encouraged in order to minimize grading and large man-made slopes.

Buildings and parking lots should be sited and designed to blend into the topography as much as possible.

The maximum gradient for slopes between development pads shall be 2:1.

3.5.5 Landscape Guidelines

The landscape guidelines in the performance standards shall apply to all private improvement

landscaped areas found in the Office Park including along roads, between lots, on all embankments or otherwise disturbed lands.

All property owners/developers shall be required to submit planting and irrigation plans at the time of the Unit Plan review.

Private improvement street-side landscaping shall follow guidelines for the particular street for grading and planting of ground cover, shrubs and trees. Shrubs shall screen parking areas. All landscaped areas are to be automatically irrigated.

Side and rear property lines shall have a 10-foot wide minimum landscaped zone. Where side or rear property lines abut streets there shall be a landscape zone in accordance with the street standards.

3.5.6 Parking

The landscape design objectives for parking lots are as follows:

- . To visually break up large paved areas with landscaping;
- . To provide shade.

A minimum of one 15-gallon tree shall be planted in the interior of the parking area for every six standard or compact spaces.

A landscape planter, minimum 5 feet wide, shall be provided at the ends of all parking rows.

Maximum grades for parking lots shall be 5%. In order to most appropriately address the varied terrain and provide safe and comfortable grades, parking lots may need to be terraced.

Parking locations and layouts shall provide for safe and convenient pedestrian circulation. Landscaped pedestrian links to the building with special paving shall be encouraged.

Standard parking stalls shall be a minimum of 9 feet by 18 feet. A 2-foot reduction in length is permitted when the vehicle front overhangs a pedestrian walk (minimum 4 feet wide) or landscaped area.

Compact parking stalls shall be a minimum of 8 feet by 16 feet. A 2-foot reduction in length is allowed when the vehicle front overhangs a pedestrian walk (minimum 4 feet wide) or landscaped area.

Forty percent of the parking area may be designed for compact cars. The number of required spaces shall comply with Chapter 16.62 of the City of Vallejo Zoning Ordinance.

Parking shall be allowed along the front curb of the building.

Where applicable, shared parking for adjacent uses (where peak use occurs at different times) will be allowed and encouraged to minimize parking lots. The amount of shared parking will be determined by the City during the Unit Plan review stage.

Access easements to allow the interconnection of parking lots should be considered. Such easements would allow the City to require connections between parking lots at a future date. Location easements shall be determined at the Unit Plan review stage.

Parking for the handicapped shall be provided in accordance with California State law.

Special closed-in parking spaces shall be provided for car and van pools.

Bicycle racks shall be provided near the building entrance.

High pressure-sodium light fixtures should be used and maximum parking lot illumination shall not exceed .5fc. Maximum standard height is 20 feet.

Lighting should be designed with attention to aesthetics and site security.

Pedestrian pathways should be illuminated separately to a maximum of 1 fc.

3.5.7 Loading and Special Equipment Areas

Service, storage, special equipment, maintenance and loading areas shall be screened from view from adjacent buildings, pedestrian areas, streets and driveways. They shall be screened from public view with planting or architectural elements. All services and loading areas will be positioned so service vehicles will not disrupt traffic flow on or off the site.

Loading areas shall be accommodated entirely on-site. Off-site vehicle loading will not be permitted.

Loading and services areas shall be screened from street frontages. Loading docks and services areas must be located on the interior building side or rear yards and concealed from public view.

When utility equipment and communication devices are located on the grounds, the site shall appear free of all such devices. Exterior utilities such as water, gas, sewage, electrical, and communication lines must be installed underground.

Refuse collections areas are to be visually screened with a solid perimeter wall. Screening wall materials and colors shall be compatible with those of the adjacent buildings. Refuse collection areas are to be located on the interior building side or rear yard.

Service, storage and maintenance areas will be constructed and maintained according to the following criteria:

- No materials, supplies or equipment, including trucks or other motor vehicles, are to be stored on-site except inside a closed building or behind an architectural screening to prevent visibility from neighboring property and streets.

All storage areas shall be screened by walls and fences and will be located on the side or rear portions of buildings.

Architectural screening shall be constructed of the same materials and finishes as the adjacent building, and shall be designed and placed to complement the building design.

No service, storage, maintenance or loading area may extend into a setback area.

3.5.8 Exterior Walls and Fences

Walls and fences shall be used for screening and to define security areas.

No freestanding walls shall be constructed within the landscape zone, unless it is used for signage and entry monumentation purposes.

Walls and fences shall be designed as an integral part of the overall site design. They will be constructed with materials that are complimentary to the style of adjacent buildings and incorporate the same finishes and colors.

No barbed wire or razor wire shall be allowed in the Commercial Center.

Chain link fencing shall not be allowed along street frontages. Where chain link fencing is proposed at other areas of the site, it shall be of a dark color and planted with shrubs and/or vines. Top and bottom rails shall be installed.

3.5.9 Signage and Entry Monumentation

All signage shall be integrated with the design, materials and colors of the building. Interior illumination and use of neon is acceptable.

Signage visible from the freeway shall not significantly impact residential areas.

Tenants or businesses in the commercial area may be identified as on a directory sign, subject to the limitations described in Sections 16.64.040K and 16.64.070 of the Vallejo Municipal Code.

Walls and signage piers should be integrated with the landscape and grading plan as part of a site feature.

All signage shall comply with Chapter 16.64, Signs, of the Vallejo Municipal Code and shall be subject to standards listed under Section 16.64.070.

The amount of signage for the commercial, business and office uses shall not be determined by parcels. Rather, the amount of signage shall be determined by the size of the overall commercial, business or office project. This is to avoid oversigning when one project may extend over several parcels.

3.5.10 Architectural Guidelines

Introduction:

The purpose of this section is to identify the principal design elements which will tie all the building types together to create a unified project. The guidelines are meant to encourage high quality architectural design. It is intended that building materials and colors be limited to help achieve an integrated design.

Color:

Buildings should be predominantly neutral in color.

Accent colors are acceptable, provided they are secondary to the overall color scheme of each building and the project as a whole. Secondary is defined as less than 10% of the total exterior wall area excluding glass.

Predominant colors and finishes should be flat.

Facade Treatment:

Buildings should have a strong visual and contemporary identity.

Buildings with highly stylized facade treatments which create historical or other inappropriate themes are discouraged. Also discouraged are reflective glass "boxes".

Spandrel elements may be accented with one or two colors, tones of the same color, or changes in texture.

Acceptable glazing colors are clear, grey tints, low reflective grey, blue or green glass. Excessive use of reflective glass or reflective metal surfaces is discouraged.

Building facades should be accented with details such as reveal joints, balcony railings, accent bands (tile, brick or concrete). Accent banding should be secondary to predominantly neutral colors of the major wall materials. If paint is used, it should be used on a different plane, rather than one color masked off from one another. These secondary facade elements can vary in material, color and detailing among buildings.

Various coursings of masonry units can be used to achieve patterns, shadows, etc..

Both horizontal window expressions and punched windows can be used for the primary themes of buildings. Limited use of curtain walls can be used for specific areas of buildings as a special treatment.

The central entry portions of the buildings should be emphasized through changing elements such as glazing, panel colors, size of accent bands, etc..

No pre-engineered metal buildings will be allowed.

Building Massing, Form and Organization:

Terracing of the building floors is encouraged to help reduce the perceived size of the buildings and bring them to a more comfortable, human scale.

Ground Floor Treatment:

The accenting of building bases should be done with attention to detailing, materials, colors, finishes, lighting and/or arcades.

Large, blank walls at building bases should be avoided.

Plaza areas adjacent to buildings should be provided for pedestrian activities. Plaza areas should be protected from the wind and solar orientation should be considered.

Desirable materials for primary pedestrian walls include pre-cast pavers, stone, brick and poured in place concrete with exposed aggregate.

Entrances should be made clearly identifiable by use of scale and material changes.

Roof:

All rooftop equipment shall be organized into major groups. Random placement of rooftop equipment shall not be allowed.

Screens and/or walls shall be used to block ground-level views within the project of all major roof equipment. The integration of mechanical equipment shall be achieved by the following methods:

- . Making the penthouse part of the building mass.
- . Using similar skin materials or colors as on building exterior.
- . Designing penthouse or equipment screens with similar forms as the building.

Exterior components of plumbing, processing, heating, cooling and ventilation systems shall not protrude above the top of any parapet.

Views of exterior components of plumbing, etc., and their screening devices, from the upper floors of adjacent buildings should be minimized by installing those elements in a compact and orderly fashion, and coloring them to blend with their backgrounds.

No exterior components of plumbing, etc., should be mounted on any building walls unless they are an integrated architectural design feature.

Recommended Design Features:

Highlighted entrance.

Decorative pedestrian plazas and walkways.

Accent landscaping and water features.

Dynamic building forms.

Light and shadow patterns.

Terracing of buildings.

Recommended Materials:

Brick

Tile with dull finish (wall and paving)

Stone (granite, marble, limestone)

Concrete

Glass (clear, tinted, low-reflective)

Exterior plaster (uniform finish, no textured patterns)

Metal panel

Wood

SECTION 4. PERFORMANCE STANDARDS

Performance Standards include guidelines which apply to the project as a whole including.

Landscape Guidelines

Fire Management

Construction Standards

Erosion Control

Noise Standards

Hazardous Waste

Glare

Odor

Air Quality

TSM

4.1 Landscape Guidelines

The Landscape Guidelines are based upon the City's Hillside Guidelines. These Guidelines apply to all lands to be landscaped, regardless of the maintenance organization.

4.1.1 General Guidelines

Landscaping and irrigation plans for all public ROWs or lands to be held in assessment districts shall be formulated by a qualified slope control specialist who is a professional landscape architect or other professional person experienced in erosion control work and revegetation.

The soils from finished grade cuts, fills and slopes shall be tested by a qualified soil laboratory to show soil type and composition, any deficiencies in plant food elements and organic matter, and Ph. The soil fertility test should include whether plants can be expected to survive. Where the inherent soil fertility is incapable of supporting plant life, as indicated in the soils engineer's report, soil amendments and/or landscape procedures to correct the infertility shall be added (organic amendments and fertilizer as specified by the soil laboratory report may be substituted for topsoil with the City's approval).

All cut and fill slopes shall be hydromulched for erosion control.

All banks or recontoured slopes shall be landscaped both for long-term erosion, fire and weed control as well as for aesthetic revegetation considerations.

All disturbed land facing public or utility rights-of-way, public open space, or at the perimeter of the subdivision shall be landscaped for erosion, fire and weed control.

In general, slopes facing public or utility rights-of-way, public open space, or at the perimeter of the subdivision shall have the greater proportion of trees and large material; special consideration shall be given to view protection and/or enhancement. However, fire safety takes priority.

All plants shall be guaranteed to be healthy and growing one year from planting, assuming appropriate care has been maintained.

Landscape plans and detailed planting specifications shall be submitted prior to final map approval and subdivision acceptance. Landscaping shall be completed according to the approved plan.

Landscape plans and detailed planting specifications shall be submitted prior to final map approval and subdivision acceptance. Landscaping shall be completed according to the approved plan.

Landscape plans shall be reviewed by the City for safety.

Due to the difficult soil conditions and slopes in the hillsides, it is critical that trees and shrubs be well-planted initially. The "flower-pot" method is recommended: this means digging out an extra-large planting hole (in cases of cut through rock, at least 3 x the width and 2 x the depth of the containers) and backfilling with a mixture of 35-50% of the original soil and 50-65% of good fertile topsoil or nitrogenized fine-grained redwood sawdust and fertilizer. A saucer shall be formed around each plant to retain water and aid erosion control.

4.1.2 Plant Selection

The project character will be established by a framework of intensive planting along the circulation corridors and in public areas throughout the project. The plant selection list has been created to ensure that the character is strong and unified.

This list is based upon the Landscape Guidelines for Blue Rock Springs Corridor, prepared by Arbogast, Newton and Griffith, Landscape Architects. The overall concept is to "use plant materials appropriate to the California oak woodland environment that naturally exists on the open slopes of the coastal mountain ranges. Most of the recommended plants fit into the "native" or "indigenous" category with some hardy ornamental plants added for variety. All recommended plants are drought tolerant and effective erosion control. All plant materials shall be considered for fire safety when planted within the 50-foot transition zone.

An excessive number of different species will not be allowed in a given composition. Plants should be combined in single organized schemes that are compatible in terms of irrigation and solar exposure purposes with similar levels of maintenance requirements.

Planting design and irrigation system design shall be submitted to the City of Vallejo for review.

This list is to be consistent with the City of Vallejo's street tree list, and shall be approved by the City.

4.1.3 General Plant List

Botanical Name:	Common Name:
Aesculus californica	California Buckeye
Arctostaphylos d. 'Howard McMinn'	Manzanita
Arctostaphylos hookeri	Monterey Manzanita
Baccharis pilularis 'Pigeon Point'	Baccharis
Baccharis pilularis 'Twin Peaks'	Baccharis
Ceanothus griseus 'Louis Edmunds'	Ceanothus
Ceratonia siliqua	Carob Tree
Cistus hybridus	White Rockrose
Escallonia 'Fradesii'	Escallonia

Eucalyptus sideroxylon, nicholii	
Fraxinus holotricha 'Moraine'	Moraine Ash
Juniperus horizontalis	
Juniperus 'sabina' 'Tamariscifolia'	Tam Juniper
Lidirodendron tulipifera	Tulip Tree
Pistache chinesis	Chinese Pistache
Platanus racemosa	California Plane Tree
Platanus x acerifolia 'Bloodgood'	London Plane Tree
Quercus varieties	Oak
Rhamnus californica	California Coffeeberry

4.1.4 Recommendations

Recommendations for trees and shrubs along edges of streets where planting area is larger than 8 feet, and where trees and shrubs can be used against fences to screen homes from the street.

Some evergreen trees, such as the tall, conical forms of the conifers:

Pinus canariensis	Canary Island Pine
Pinus halepensis 'Brutia'	
Pinus halepensis	Aleppo Pine
Pinus muricata	Bishop Pine
Sequoia sempervirens	Coast Redwood

Some native broadleaf evergreen trees such as:

Lithocarpus densiflorus	Tan Oak
Quercus agrifolia	Coast Live Oak
Schinus molle	California Pepper
Umbellularia californica	California Bay

Accent trees:

Ginkgo biloba	Ginkgo
Liquidambar styraciflua	Sweetgum
Malus sp.	Crabapple
Populus nigra 'Italica'	Lombardy Poplar
Prunus sp.	Flowering Cherry

Shrubs for screening and color:

Acacia longifolia	Sydney Golden Wattle
Ceanothus arboreum 'Ray Hartman'	Ray Hartman Ceanothus
Eleagnus pungens 'Fruitlandii'	Eleagnus
Fremontodendron c. 'California Gold'	'California Gold'
Herteromelis arbutifolia	Toyon
Nerium oleander	Oleander

Tall Shrubs:

Acacia longifolia	Sydney Golden Wattle
Ceanothus arboreum 'Ray Hartman'	Ray Hartman Ceanothus
Eleagnus pungens 'Fruitlandii'	Eleagnus
Fremontodendron c. 'California Gold'	'California Gold'
Herteromelis arbutifolia	Toyon
Nerium oleander	Oleander

Medium Shrubs:

Arctostaphylos d. 'Howard McMinn'	Ceanothus
Ceanothus griseus 'Louis Edmunds'	White Rockrose
Cistus hybridus	Orchidspot Rockrose
Cistus purpureus	Madeira Rose
Cytisus maderensis	Escallonia
Escallonia 'Fradesii'	Pittosporum
Pittosporum tobria	

Low Growing Shrubs:

Arctostaphylos hookeri	Monterey Manzanita
Ceanothus	
Hebe 'Coed'	Hebe
Juniperus conferta	Shore Juniper
Juniperus sabina 'Tamariscifolia'	Tam Juniper

Ground Covers:

Arctotheca calendulacea	Yellow
Atriplex semi-baccata	Salt Bush
Ceantranthus ruber	Red Valerian
Erigonium fasciculatum	California Buckwheat
Gazania leucoleana	Trailing Gazania
Gazania splendens hybrids	
Hedera helix	English Ivy
Hypericum calycinum	Aaron's Beard
Myoporum parvifolium	
Pelargonium peltatum	Ivy Geranium
Vinca major	
Vinca minor	Periwinkle

4.1.5 PG&E Easement Planting

All planting under the PG&E easement is subject to review and approval by PG&E. PG&E may approve other plants not listed, if they have the assurance that the plants can be maintained within their height limitations. Trees that exceed the height limitations at maturity may be utilized if they can be maintained to conform within the regulations.

The following plant list recommends trees and tall shrubs that are suitable for these conditions. They have been selected because of their proven success at other sites with similar environmental conditions and for their ability to be maintained within the limitations of the easements without losing their interesting form or character. These plants will be maintained under agreement with PG&E.

Plant List for PG&E Easement:

Botanical Name:	Common Name:
Arbutus unedo	Strawberry Madrone
Acacia longfolia	Sydney Godlen Wattle
Acacia melanoxylon	Black Acacia
Escallonia species	Escallonia
Eucalyptus lehmanii	Bushy Yate
Photina fraserii	Photina
Myrica californica	California Wax Myrtle
Cercis occidentalis	Western Redbud
Heteromeles arbutifolia	Toyon
Fremontidendron californicum	Flannel Bush
Ceanothus sp.	Ceanothus
Arctostaphylos sp.	Manzanita
Aesculus californica	California Buckeye
Rhamnus californica	California Coffeeberry
Dodonea viscosa	Hopseed Bush

4.1.6 Fire Management

Properties adjacent to unirrigated open space are particularly vulnerable to grass fires. Certain plant materials and planting practices can reduce the potential of fire hazards.

Standards:

All properties located adjacent to large unirrigated open spaces shall be required to maintain a 50-foot zone in accordance with the City of Vallejo's Weed Abatement Ordinance and Fire Department policy. This includes a 25-foot irrigated landscape zone plus a 25-foot zone which is cleared, cut or mowed by May 1 of each year.

An irrigation plan and landscape plan shall be submitted for subdivision approval. All landscaping within the transition zone shall be approved by the Fire Department for safety. It is recommended to plant only groundcover for optimum safety.

Trees shall not be planted within 30 feet of the structure where the yard abuts open space.

Planting around the house should be fully irrigated.

The following plant materials are not allowed in the 50-foot fire break zone because of their flammability:

Acacia
Cedar
Cypress
Eucalyptus
Juniper
Pine

All plant materials shall be selected by their "low-fuel volume"¹ and ability to control erosion.

All vegetation in the fire break zone shall be maintained to the Fire Department's standards.

4.2 GRADING GUIDELINES

4.2.1 General Guidelines

No area is to be graded before a soils test and report are conducted by a by a qualified soils engineer. Grading operations shall under the supervision of a qualified soils engineer, and sufficient time shall be spent by the soils engineer on the site to certify the proper treatment of all cut and fill and to the adherence of all other requirements for which he certifies.

All grading shall comply with the City of Vallejo Grading Ordinance and the recommendations of the FSEIR, as stated on pages 3.5-9 through 3.5-12 of the "Comments and Responses" volume.

Each area planned for development is to have a grading and erosion control master plan before an individual parcel can be graded.

Cut/fill slopes will be left at a safe angle of repose based on the soils report, and in accordance with the recommendations of the soils engineer.

¹ The following are references for appropriate planting chaparral/urban interface:

Department of Agriculture, Forest Service. "A Homeowner's Guide to Fire and Watershed Management at the Chaparral/Urban Interface". National Foundation for Environmental Safety, Inc., 2210 Wilshire Blvd., Suite 184, Santa Monica, CA 90403.

Ibid. "Living More Safely in Chaparral/Urban Interface". General Technical Report, PSW-67. Pacific Southwest Forest and Range Station, P.O. Box 245, Berkeley, CA 94710.

In some cases, at the edges of the developed areas where proposed contours meet existing contours, it may be necessary and/or desirable to use 2:1 slopes in order to minimize the disturbances of natural landforms or offsite features. Additionally, where existing horizontal and vertical road alignments and intersection locations dictate roadway alignments within the proposed development, 2:1 slopes may be necessary in order for proposed roads to conform with the existing road configuration.

These exceptions to the slope gradient standards shall be subject to the approval of the City on a case-by-case basis where the need is demonstrated. On the Preliminary Grading Plan, 2:1 slopes are proposed for the following locations within the single family residential areas:

- . Where Redwood Parkway enters the project, east of the Redwood/Ascot intersection, 2:1 slopes are necessary adjacent to the roadway so as to create minimal disturbance to the hillside on which the water tank is located.
- . At the southern property boundary, along the golf course, 2:1 slopes are necessary in order to keep fill to a minimum where it encroaches near the golf course.
- . Where housing overlooks the Commercial Center.

Where suitable topsoil exists on areas to be disturbed by grading or building operations, the topsoil shall be stripped and stockpiled in convenient locations for storage during construction for later use in landscaping and erosion control.

Variable slopes will be used to mitigate the environmental and visual impacts of grading. Wherever possible, slopes will be constructed with contoured shapes and have rough, irregularly finished surfaces. By contouring the face of a slope, the resulting differences in sun exposure and varied drainage will support a variety of plant growth. This would enhance the current topographic character of the site. Significant trees, rock outcroppings, and other unique landscape features should be preserved wherever possible. Abrupt angles for cut and fill slopes shall be avoided.

The angle of a graded slope shall be adjusted so that a smooth transition from graded slope to natural terrain is created.

Points of intersection of natural and graded slopes shall be rounded to provide a smooth, natural appearing transition.

The horizontal contours of proposed slopes shall be curved in an undulating fashion with varying radii to reflect the natural terrain.

Terraces of 6 feet minimum width would occur every 30 feet of height in areas where slope exceeds 3:1.

No cut/fill slopes shall be abandoned for extensive periods of time without provisions for erosion protection and adequate drainage, as approved by the City. During construction, erosion control

would be in place by October 15 of each year, followed with provision for continuing maintenance.

Positive drainage shall be achieved in all improved areas so that water flows away from structures.

All cut/fill slopes regardless of height, steeper than 5:1, would have an erosion control and maintenance program (hydromulching, interceptor drains, planting pockets, etc.).

All cut/fill slopes shall have a 2-foot minimum flat area at top and toe of slope when adjacent to bicycle paths, walks and roadbeds. Property lines along the tops of slope of open space parcels shall be set back three feet from the top of slope, to provide a level bench for fence maintenance and to accommodate potential erosion.

Pad cross-pitch shall be at least 1% for positive drainage.

4.2.2 Guidelines for maximum gradients

5% Parking lots, extended pedestrian paths, except in unusual circumstances subject to design review.

8.33% Handicap ramps.

10% Driveways.

4.3 CONSTRUCTION STANDARDS

4.3.1 Roads

All streets shall have curbs and gutters that conform to City of Vallejo design standards. Vertical and horizontal alignments, and pavement sections shall conform to City of Vallejo standards.

Oil screens, if required by Vallejo Sanitation and Flood Control District, shall be incorporated into the storm water system of roads and parking lots.

Maintenance: Streets shall be cleaned and swept on a monthly basis by the City of Vallejo. Upon completion of the project and review by the City, the area shall be adopted into the Sweeping Plan.

4.3.2 Bikeway System

In accordance with the City of Vallejo Master Plan, a bikeway system shall perform the dual function of providing bike routes to places of work and to and from residences, as well as provide for scenic recreational bicycling.

Bicycle paths and lanes shall follow State guidelines for signing, striping, paving and providing tire proof grates over inlets. Bikeways shall occur on major roads and allow for bicycle access

to all development areas. The City of Vallejo and GVRD should review all bicycle and GVRD should review all bicycle paths and lanes.

4.3.3 Pedestrian Circulation

Design standards shall conform to the City of Vallejo Municipal Code. Crosswalks with adequate markings and signage devices shall be designated by the traffic engineer.

All major intersection, and where otherwise required, shall have pedestrian crosswalks. Special entry streets shall have accent paving to define pedestrian crossings.

Barrier free pedestrian movement with handicap ramps at all intersections shall be required to meet State guidelines.

A pedestrian linkage shall connect various points within the project area to the Blue Rock Springs Corridor.

4.3.4 Transit Stops

Transit stops shall be provided as required and shall be designed to the standards established by the City of Vallejo and Vallejo Transit. Refer to Section 2.2.10 for bus stop guidelines.

4.3.5 Construction

All projects shall conform to the State of California Seismic Standards, Title 24 (handicapped code), the Uniform Building Code and the City of Vallejo Standards.

The project shall conform to all City handicap requirements.

The project shall comply with the Uniform Building and Uniform Fire Codes and with the requirements of the Fire Department's development standards.

4.3.6 Flood Damage Protection Ordinance

All development shall comply with the Flood Damage Protection Ordinance.

The project shall conform to Vallejo Sanitation and Flood Control District's Master Plans for storm drain and sanitation sewer improvements, and, if applicable, to VSFCD 4138.

4.3.7 Water and Wastewater

The project shall comply with the City of Vallejo's Water System Master Plan based upon the project's water demand.

Water conservation measures shall be incorporated into all aspects of the project.

The wastewater system shall incorporate low flow fixtures and other features approved by VSFCD to reduce wastewater generation.

4.3.8 Utilities

Natural gas and electricity shall be provided by PG&E.

The project shall participate in any PG&E programs for reducing energy consumption when feasible.

Telephone service shall be provided by Pacific Bell.

Cable television shall be provided by Donrey or the current City franchise.

4.4 EROSION CONTROL

A detailed temporary erosion and sedimentation control plan for each phase of construction will be prepared and submitted for review and approval by the City, as stipulated in the Grading Ordinance. The guidelines for such plans will be as follows:

- . Locate staging areas outside major streams and drainage-ways.
- . Keep slope lengths and gradients to a minimum.
- . Discharge construction runoff into small drainages at frequent intervals to avoid build-up of large potentially erosive flows.
- . Minimize disturbed areas.
- . Keep runoff away from disturbed areas during construction.
- . Stabilize disturbed areas as quickly as possible, either by vegetative or mechanical methods.
- . Keep storm water runoff velocities low.
- . Trap sediment before it leaves the site with such techniques as check dams, sediment ponds or siltation basins.

4.5 NOISE

Construction work shall be limited to daylight hours and all equipment and operations with a high noise potential should be muffled or controlled, as feasible.

Noise levels from construction equipment shall meet the City of Vallejo's Performance Standards in the Zoning Ordinance.

Appropriate acoustic insulation shall be provided as necessary to protect new land uses and to mitigate project-generated noise impacts on adjacent areas.

4.6 SOLID WASTE MANAGEMENT

All hazardous materials shall be stored, handled, transported and disposed of in accordance with the Vallejo Fire Department standards, the Solano County Hazardous Waste Management Plan and all State and Federal regulations.

Solid waste disposal shall be provided by Vallejo Garbage Service or the current City franchise.

4.7 GLARE

Glare should be considered in the siting of buildings and signs.

4.8 ODOR

All land uses shall be required to conform to all State and Federal air quality standards regarding odor.

4.9 AIR QUALITY

Temporary Impacts:

During grading, excavation and construction activities, exposed earth surfaces shall be watered at least twice a day; once in late morning and at the end of the day. The frequency of watering should increase if wind speeds exceed 15 mph.

Conditions of approval shall require daily cleanup of mud and dust carried onto street surfaces by construction vehicles.

Throughout excavation activity, haul trucks shall use tarpaulins or other effective covers.

Upon completion of construction, contractors shall take measures to reduce wind erosion. Replanting and repaving shall be completed as soon as possible.

Long Term Impacts:

Implementation of a Transportation Systems Management (TSM) ordinance or program could help reduce emissions by reducing peak period generation by the project. Each developer is encouraged to participate in a TSM program.

4.10 TRANSPORTATION SYSTEMS MANAGEMENT (TSM) PROGRAM STANDARDS

If the City does not implement a citywide Transportation System Management ordinance within two years of adoption of the Northgate SAP, then the project sponsor shall develop a TSM program within three years of adoption of the SAP. Such program should include information regarding the availability of the following:

- . Linkages between the project and the San Francisco-Vallejo ferry service;
- . Linkages between the project and BART;

- . Bus shuttles within the project area;
- . Van pools to employment centers outside the project area.

4.10.1 Goals and Measures

The primary goal of a TSM program for Northgate should be to reduce the number of single occupant automobiles driven to employment sites in order to mitigate air quality impacts and traffic congestions.

A 3-acre Park and Ride lot has been identified within the SAP north of Columbus Parkway at Ascot Parkway. This lot will be developed to serve Vallejo residents who are I-80 commuters.

The following are additional voluntary measures to be encouraged:

- . Preferential parking for vanpools and carpools, and bicycles.
- . Flexible and staggered working hours.
- . Distribution of transit information on site.
- . Employee incentives for carpooling and transit use.
- . Appointment of a transportation coordinator to promote and overview the program.
- . Encourage participation in RIDES program and carpool matching programs.
- . Bus stops/shelters.
- . Bike lanes and racks.

4.10.2 City Commitments

In adopting a TSM ordinance for new and existing developments, the City of Vallejo should undertake the following tasks:

- . Appointing a transportation coordinator to work with employers and the public to promote and monitor the program.
- . Conducting an annual survey to establish employee commute patterns. The survey would be conducted through employers.
- . Monitoring requirements (annual report from each developer).
- . Requiring a TSM program from each developer during the project review stage.
- . Establishing threshold levels WHEN mandatory TSM standards may be imposed.

APPENDIX B:
PUBLIC POLICIES

NORTHGATE SPECIFIC AREA PLAN

This document is under separate cover, and is on file in the Planning Division,
City of Vallejo.

APPENDIX C:

CONDITIONS OF PROJECT APPROVAL

NORTHGATE SPECIFIC AREA PLAN

NORTHGATE VESTING TENTATIVE MAP

SPECIFIC PLAN #88-01

CONDITIONS OF APPROVAL

October 25, 1988

Note: Most of these conditions have been incorporated into the Specific Area Plan included herein.

EXHIBIT A
CONDITIONS OF APPROVAL
SPECIFIC PLAN #88-01

Adopted by the Vallejo City Council on October 25, 1988

(Note: The term "project sponsor" is defined as the applicant or its successors in interest.)

A. Prior to the acceptance of the first subdivision map application:

1. The project sponsor shall revise the Northgate Specific Area Plan (June 1988) to include the following:
 - a. Include as a SAP goal that adequate circulation shall be built into the project so as not to impact existing development.
 - b. The "Northgate Specific Area Plan, Traffic and Transportation Element" by DKS Associates, dated June 1988, shall be incorporated into the SAP. The following corrections shall be made in this element:
 - 1) The signalization of Admiral Callaghan Lane and Tennessee Street will be completed by the City by 1989 (page 2, paragraph 1).
 - 2) Traffic at the Carquinez Toll Plaza experiences excessive delays in the eastbound direction during the p.m. peak hour, not westbound (page 4, paragraph 2).
 - 3) West of SR 29, SR 37 has two lanes eastbound, one lane westbound with one two-way left turn lane (page 4, last paragraph).
 - 4) On Figure II-1, page 5, show the signals at Ascot and Columbus Parkways and at Tennessee Street and Admiral Callaghan Lane.
 - 5) The intersection of Columbus Parkway and Ascot Parkway south is currently controlled by a traffic signal (page 6, paragraph 3).
 - 6) The new off-ramp at Redwood Parkway and Admiral Callaghan Lane will be restricted to right turns only (page 11, paragraph 5).
 - 7) There is also an existing bike path along the Blue Rock Springs Creek from the Ascot bridge to Hanns Park (page 14, paragraph 3).

- 8) Intersection #11 should have two lanes for northbound to eastbound right turns only.
- c. The SAP shall be revised to be internally consistent in all respects with the "Northgate Specific Area Plan, Traffic and Transportation Element" by DKS Associates, dated June 1988.
- d. Make the following changes within the SAP text:
 - 1) On the first page of the Table of Contents, add: "7.2 Fire Prevention."
 - 2) On page 6, under 1.5.8, add the following underlined text: "Controlled access will be limited..." and "Controlled access will be provided..."
 - 3) On page 8, last paragraph, add the following: "Because the site has been heavily grazed, consideration may be given to the reestablishment of native species."
 - 4) On page 11, first paragraph, add the following underlined text: "...should move occur, or, if required, such landslides will be repaired."
 - 5) On page 14, under 1.7.4, add the following underlined text: "...from the Cache Slough Diversion and the Barker Slough Diversion."
 - 6) On page 14, under 1.7.4, add the following underlined text: "...has a present capacity of one million gallons of potable water," and "...has a capacity of one-half million gallons of untreated water for golf course and park irrigation."
 - 7) On page 17, under 2.3, delete the third paragraph and replace with the following: "Locations of the Business, Office and Commercial areas are placed where they will have the fewest impacts on residential uses and traffic circulation, and where they will be most successful for the economic development of Vallejo."
 - 8) On page 18, revise the second and third paragraphs to the following:

"The uses that will be allowed in the Office Park with an approved Planned Development unit plan:

Administrative and professional services;
Business equipment sales and services;
Business support services;
Communication services;
Convenience sales and personal services;
Eating and drinking establishments, if not described below;
Finance, insurance and real estate services;
Medical services;

Personal services of functional community training for developmentally disabled;
Research services;
Day care facilities;
Retail sales associated with office uses;
Essential services.

The uses that will be allowed in the Office Park with an approved Major Use Permit:

Eating and drinking establishments, with on- and/or off-site sale of alcoholic beverages;
Participant sports and recreation;
Major impact utilities and services.

No industrial or residential uses will be allowed.
No fast food restaurants or drive-up windows will be allowed."

- 9) On page 19, fifth paragraph, change. 32 acre Business Park to 67.1 acre Business Park.
- 10) On page 19, revise the third and fourth paragraphs to the following:

"The uses that will be allowed with an approved Planned Development Unit Plan in the Business Park are the same as the Office Park with the addition of the following:

Construction sales and services;
Laundry services;
Repair services;
Wholesale storage and distribution;
Horticultural materials and services;
Light manufacturing and assembly;
Day care facilities.

The uses that will be allowed with an approved Major Use Permit in the Business Park are the same as the Office Park with the addition of the following:

Automotive and equipment: sales/rentals, light equipment.

No residential or heavy industrial uses will be allowed. No fast food restaurants or drive-up windows will be allowed.

Any of the allowable uses which conducts all or part of its business outdoors on its site and/or which has outdoor storage shall screen all such outdoor activities from the public right-of-way and adjacent properties."

- 11) On page 20, revise the third paragraph to the following:

"The uses that will be allowed with an approved
Planned Development unit plan:

Administrative and professional services;
Animal sales and services (indoor);
Building maintenance service;
Building supplies;
Business equipment sales and services;
Business support services;
Communication services;
Convenience sales and personal services;
Cultural exhibits;
Eating and drinking establishments, if not
described below;
Finance, insurance and real estate services;
Food and beverage retail sales, if not
described below;
Garden supplies;
Laundry services;
Lodging;
Participant sports and recreation (indoor);
Personal services, functional community training
for the developmentally disabled;
Personal services, general;
Postal services;
Repair services, consumer;
Retail sales;
Wholesale sales when associated with
retail sales
Day care facilities;
Essential services.

The uses that will be allowed with an approved Major
Use Permit:

Automotive sales;
Eating and drinking establishments, with
on- and/or off-site sales of alcoholic
beverages;
Food and beverage retail sales, with on- and/or
off-site sales of alcoholic beverages;
Gasoline sales;
Participant sports and recreation (outdoor);
Major impact utilities and services.

No residential, except lodging, or industrial uses
will be allowed."

- 12) On page 21, under 6, second paragraph, add the following:
"In addition, community education and neighborhood recre-
ation facilities will be permitted."
- 13) On page 26, under 7.7, add the following: "This facility
will be landscaped to mitigate visual impacts, but will

still allow surveillance for security persons from Columbus Parkway."

- 14) On page 27, first paragraph, delete "voluntary."
- 15) On page 27, revise the third paragraph to: "The Traffic and Transportation Element (June 1988) is incorporated into this SAP."
- 16) On page 32, under 8.4, add the following underlined text: "Bus stops and bus turnouts with bus passenger shelters will also be incorporated..."
- 17) On page 32, under 9.2, add the following underlined text: "...provide erosion control while protecting views for residential lots."
- 18) On page 32, under 9.2, add the following underlined text: "...with the 25' irrigated landscape buffer and 25' disced fire zone or 50' disced fire zone for lots adjacent to the open space."
- 19) On page 33, under 9.2, add the following underlined text: "...help insure the privacy and security of the adjacent..." and "...open space shall be 2:1 with vegetation as recommended by the City's Landscape Guidelines for Hillside Development."
- 20) On page 33, under 9.3, delete all references to "dedication of the property to the County."
- 21) On page 39, delete text under 11.4.2, and replace with the following:

"The zoning of the project area will be changed to MUPD, Mixed Use Planned Development. Once this zoning is in place, the SAP will serve as the Planned Development Master Plan for project and as the zoning requirements and standards."
- 22) On page 39, add the following underlined text: "...rights and responsibilities of each party and all successors-in-interest during the development..."
- 23) On page 41, change the method of financing for maintaining on- and off-site water facilities to "Water Revenues."
- 24) On page 42, under 12, add the following text: "The steps within the implementation process may change if the City determines such change is in the best interest of the City and is consistent with local ordinances."
- 25) On page 42, under 12, #4, delete the text and replace with the following: "Development agreements may be entered into by Bedford Properties (or successors-in-interest) and the City of Vallejo."

- 26) On page 43, item 5, change "parcel map" to "final map."
- 27) On page 1, Appendix A, under 2.1, add the following underlined text: "...of the Bay Area and is drought tolerant."
- 28) On page 2, Appendix A, third paragraph, add the following underlined text: "All public improvements shall be constructed to the City of Vallejo's and Vallejo Sanitation and Flood Control District's standards and are subject to the City's and VSFCD's approval."
- 29) On page 7, Appendix A, delete the last paragraph under 2.2.4.
- 30) On page 11, Appendix A, change "parking on one side of roadway" to "parking on both sides of roadway."
- 31) On page 12, Appendix A, under Residential Cul-de-Sac Standards add: "40-foot radius of cul-de-sac."
- 32) On pages 15-16, Appendix A, combine 3.1 with 2.2.11 or establish a new section of development standards for open space that does not distinguish between public and private open space. In whatever format is used, delete 3.1.5.
- 33) On page 16, Appendix A, under 3.2, add the following: "Garage conversions shall not be permitted."
- 34) On page 17, Appendix A, under Maximum Coverage, change "or" to "and."
- 35) On page 17, Appendix A, delete all the text under Building Setbacks, and replace with the following:

"Each building or structure shall be set back the following minimum distances:

Corner lots: Set backs from the street (non-garage side) shall be subject to review by the Planning Division. Set back from the property line to the garage door will be a minimum of 20 (twenty) feet. The rear yard shall have a minimum depth of 20 (twenty) feet as measured from the rear of flat useable area not exceeding 10% slope. Minimum distances between structures will be 10 feet or subject to review by the Planning Division during submission of subdivision and unit plan applications.

Interior Lots and Through Lots: Set back from the property line to the garage door will be a minimum of 20 (twenty) feet. Minimum distances between structures will be 10 feet and/or subject to review by the Planning Division during submission of subdivision and unit plan applications. The rear yard shall have a minimum depth of 20 (twenty) feet, as measured from

the rear portion of the house, of flat useable area not exceed 10% slope."

- 36) On page 18, Appendix A, under Fencing, add the following: "Fencing along the top of slopes of open space parcels shall be set back three feet from the top of slope to accommodate potential erosion of the slope."
- 37) On page 18, Appendix A, under Fencing, change "two fence types should be avoided" to "two fence types shall be avoided."
- 38) On page 19, Appendix A, under 3.2.5, add the following underlined text: "...shall be provided and installed by the developer..."
- 39) On page 19, Appendix A, under Subdivision Entries, delete the text and replace with the following:

"Subdivision identification signage may be provided at the access road into each of the seven low density residential neighborhood. This signage shall not exceed 24 square feet per neighborhood and shall not have interior illumination. Additionally, entries will be defined by one or more of the following: crosswalks with accent paving, entry walls, entry gates sculptures and special plantings. Such signage shall not block the line of sight of traffic entering or leaving the neighborhoods."
- 40) On page 25, Appendix A, revise 3.4.1 to add the following underlined text: "Primary Uses Permitted with a Major Use Permit."
- 41) On page 31, Appendix A, under 3.5.7, and on page 40, Appendix A, under 3.6.6, add the following standard: "Entrances to private property from a public street shall be a driveway approach as per City of Vallejo standards."
- 42) On page 31, Appendix A, under 3.5.8, last paragraph, change "should" to "shall."
- 43) On page 38, Appendix A, under 3.6.3, change all "shoulds" to "shalls."
- 44) On page 39, Appendix A, delete the sixth paragraph.
- 45) On page 48, Appendix A, under 3.7.9, delete the fourth paragraph, and add the following: "Tenants or businesses in the commercial area may be identified as on a directory sign, subject to the limitations described in Sections 16.64.040K and 16.64.070 of the Vallejo Municipal Code."
- 46) On page 48, Appendix A, under 3.7.9, delete the sixth paragraph and replace with: "All signage shall comply with Chapter 16.64, Signs, of the Vallejo Municipal Code

and shall be subject to the standards listed under Section 16.64.070."

- 47) On page 53, Appendix A, under 4.1.2, add the following underlined text: "...schemes that are compatible in terms of irrigation and solar exposure purposes with similar levels of maintenance requirements."
 - 48) The General Plant List on page 53 of Appendix A shall be revised to reflect the City's street tree list and to meet the approval of the City.
 - 49) On page 58, Appendix A, last paragraph, add the following: "Property lines along the tops of slope of open space parcels shall be set back three feet from the top of slope, to provide a level bench for fence maintenance and to accommodate potential erosion."
 - 50) On page 59, Appendix A, under 4.3.1, add the following underlined text: "All streets shall have curbs and gutters that conform to City of Vallejo design standards. Vertical and horizontal alignments, and pavement sections shall conform to City of Vallejo standards."
 - 51) On page 61, Appendix A, under 4.6, add the Solano County Hazardous Waste Management Plan to the list.
 - 52) Revise 4.10, Appendix A, to reflect Condition F.19.
 - 53) Figures 19 and 20 are reversed. Figure 17 should be labeled "Typical Residential Street" and Figure 20 should be labeled "Typical Residential Collector Street."
- e. Add the following standards into the appropriate sections of the SAP:
- 1) Certain agricultural uses, such as grazing, shall be permitted in the permanent open space areas where feasible and where allowed by the City of Vallejo.
 - 2) All grading shall comply with the City of Vallejo Grading Ordinance and the recommendations of the FSEIR, as stated on pages 3.5-9 through 3.5-12 of the Comments and Responses volume.
 - 3) Defensible space and traffic safety design techniques shall be used in all aspects of the project.
 - 4) Views of the project area shall be enhanced and protected with natural land forms, adequate landscaping and screening, and quality architectural design of all structures.
 - 5) Berms shall be used where feasible to screen parking lots from public rights-of-way.

- 6) Landscape areas shall provide adequate visual access to entrances and exits of all structures.
- 7) Those residential lots that are adjacent to the golf course shall include landscaping, fencing or other methods deemed appropriate by the City to reduce the impacts of golf balls on the residents. In addition, the CC&R's shall include language that will indemnify the City and the Vallejo Golf Club from residential property damage from golf balls.
- 8) Recreational and/or neighborhood amenities shall be incorporated where feasible into the low density residential neighborhoods.
- 9) Residential units shall not be permitted to be used for commercial purposes, except as provided for in Chapter 16.60, Home Occupations, of the Vallejo Municipal Code.
- 10) If possible, the uses within the commercial area should include well-known retailers, a department store, and provide goods and services not currently available in Vallejo. Neighborhood services should be limited.
- 11) Noise mitigation measures shall be used on Columbus, Ascot, Turner and Redwood Parkways when traffic noise levels exceed City of Vallejo standards.
- 12) The traffic mitigation measures described in Figure IV-1 of the Supplement volume of the FSEIR shall be incorporated into the project and shall be the responsibility of the project sponsor. Exceptions to these mitigation measures may be made if the City, CalTrans and/or project sponsor can demonstrate traffic impacts can be lessened further using alternative means.
- 13) Driveway access onto major streets shall be limited.
- 14) A "Not a Through Street" shall be posted at the access road to residential area "A." The street name shall not be "Turner."
- 15) The street standard for Doris Drive shall depend on the development on either side of the street and the points of access to serve such development.
- 16) The parking or storage of trailers, recreational vehicles, boats and other special purpose limited use vehicles shall only be permitted in residential areas when such vehicles are screened from public view.
- 17) Bicycle racks shall be provided in conjunction with the parking areas for all nonresidential uses and with the park-and-ride facility.

- 18) A pedestrian linkage shall connect various points within the project area to the Blue Rock Springs Corridor.
 - 19) The project shall conform to all City handicap requirements.
 - 20) The project shall comply with the Uniform Building and Uniform Fire Codes and with the requirements of the Fire Department's Development Standards.
 - 21) The project shall conform to Vallejo Sanitation and Flood Control District's Master Plans for storm drain and sanitary sewer improvements, and, if applicable, to VSFCD Resolution 4138.
 - 22) The wastewater system shall incorporate low flow fixtures and other features approved by VSFCD to reduce wastewater generation.
 - 23) Water conservation measures shall be incorporated into all aspects of the project.
 - 24) The project shall comply with the City of Vallejo's Water System Master Plan based upon the project's water demand.
 - 25) Natural gas and electricity shall be provided by Pacific Gas & Electric Company.
 - 26) The project shall participate in any PG&E programs for reducing energy consumption when feasible.
 - 27) Solid waste disposal shall be provided by Vallejo Garbage Service or the current City franchise.
 - 28) Telephone service shall be provided by Pacific Bell.
 - 29) Cable television shall be provided by Donrey or the current City franchise.
 - 30) Within the project area there shall be no net loss of wetland habitat as a result of the project.
- f. Change all references in the SAP of "Urban Reserve" to "undeveloped Open Space."
 - g. Change all references in the SAP of "senior housing" to "high density units."
 - h. The "Undesignated" area on the project plan shall be changed to "Urban Reserve." This designation shall remain until such time as the adjacent properties (known as the N.D.S. and Olson properties) are proposed for development. Then the 6-acre piece shall be considered for development, and shall undergo additional environmental review as required by the City.

- i. Revise page 14, paragraph 1.7.1 to accurately reflect VSFCD improvements.
- j. Include in the SAP a street section which meets the approval of the Public Works Department and the Planning Division for the former Turner Parkway right-of-way (through residential area "A") between the Turner Parkway/Foothill Drive intersection and Ascot Parkway.
- k. Revise page 25, paragraph 7.5, to the following:

"A high school site will be purchased by the school district, through a Mello-Roos District. The site shall yield a minimum of 40 acres to meet State and VCUSD standards. The site is at the corner of Ascot Parkway and Redwood Parkway. The site is to be combined with the Blue Rock Springs corridor to the south and linked to this lineal parkway by walks and bikeways."
- l. If necessary, residential area "F" shall be modified to insure the high school site is a minimum of 40 usable acres.
- m. Revise the SAP to increase the amount of commercial area across Doris Drive, and to reduce the amount of office and/or business uses. All the uses and standards applicable to the commercial area on the west side of Doris Drive shall be applicable to the new commercial area on the east side of Doris Drive.
- 2. The project sponsor and the Vallejo City Unified School District shall resolve the terms of purchase and improvement of the high school site through the Mello-Roos district.

B. Prior to approval of the first tentative map:

- 1. The project sponsor shall enter into an agreement with the City that will guarantee the provision of on-site and off-site infrastructure required to serve the project by the project sponsor and/or successors-in-interest.
- 2. The project sponsor, the City, GVRD and/or other appropriate agencies and groups shall determine the ownership and maintenance responsibilities of all permanent open space within the project area.
- 3. The project sponsor shall dedicate/provide the fire station site to the City.
- 4. The project sponsor and the City shall further refine the planned improvements at Turner Parkway/Foothill Drive and Ascot Parkway/Area "A" access intersections to insure traffic impacts are reduced to the lowest level feasible.

C. Prior to approval of the first grading permit:

- 1. Additional studies shall be undertaken and shall include the following:

- a. Geologic mapping and subsurface investigation with recommendations for mitigating any slope instabilities, landslide conditions, and liquefiable materials.
- b. Analysis and recommendations for mitigating any groundwater hazards.
- c. Identification of bedrock orientations and other stability conditions for each cut slope.

These studies shall be reviewed and approved by the Public Works Department prior to authorizing any financing. The project sponsor shall follow all recommendations of such studies, as required by the City.

2. Grading plans for work in the vicinity of PG&E towers shall be reviewed by PG&E.
3. A civil engineering vibration specialist shall conduct a one-time analysis of the attenuation characteristics of the vibrations produced by each type of construction/excavation equipment or activity for each site subjecting existing structures to such vibrations.
4. A detailed hydraulic and water quality study shall be prepared based upon the storm drainage Master Plan of VSFC. This study shall include the following:
 - a. Recommendations for the realignment of creeks to satisfy the requirements of VSFC.
 - b. Landscaping and grading standards that will reduce the potential for downstream flooding.
 - c. Standards for drainage patterns and flow paths that will result in infiltration and slower stormwater velocities.
 - d. Analysis of potential water quality impacts to Lake Chabot and measures to mitigate any such impacts.
 - e. Recommendations for the use of permanent sedimentation basins or low velocity flows within the upper reaches of the creeks.
 - f. Standards for the use of fertilizers, pesticides and other hazardous materials.
 - g. Erosion control recommendations.

This study shall be reviewed and approved by the City, VSFC and all other appropriate agencies and groups.

5. Permits from the U.S. Army Corps of Engineers and Department of Fish and Game (and Regional Water Quality Control Board, if required) shall be secured.
6. For the review and approval of the City and the Vallejo Sanitation and Flood Control District, a detailed temporary erosion and

sedimentation control plan shall be prepared for each construction and operational phase of the project to meet the standards prescribed by ABAG.

7. The maintenance responsibility for the stormwater system and any sedimentation basins and subdrains during the construction and operation phases shall be established to meet the requirements of the Vallejo Sanitation and Flood Control District.
8. The topsoil storage area location(s) shall be approved by the Public Works Department and Planning Division, and shall be located where feasible adjacent to the location(s) where the topsoil will be used in the final landscaping. These locations shall not be adjacent to existing or future residential areas.
9. The alteration of Rindler Creek shall be reviewed and approved by the Army Corps of Engineers, Department of Fish and Game, Regional Water Quality Control Board, Vallejo Sanitation and Flood Control District and any other permitting agencies. The plans for such alteration shall include the following if approved by the above agencies:
 - a. Rock lining shall be used only in areas where a high erosion potential exists.
 - b. Check dams, gabion structures and increased creek widths shall be used where appropriate.
 - c. A diversity of native wetland plant species should be used in a three layer development of plant structure.
 - d. The Revegetation Manual published by the Alameda Flood Control District shall be used as a design guide.
10. The project sponsor shall post a performance bond.

D. Prior to approval of the first final map or improvement plans, whichever occurs first:

1. A consent agreement shall be secured from PG&E prior to use of transmission rights-of-way.
2. The project sponsor shall consult with the Vallejo Sanitation and Flood Control District regarding how project-related improvements can lessen any project-related impacts on the District.
3. A maintenance program for Rindler Creek shall be prepared, and shall meet the requirements of the City, VSFC, Corps of Engineers and the Department of Fish and Game. This program shall identify maintenance requirements and responsibilities.
4. If required by the City, the project sponsor shall provide a repeater site and equipment.

5. The project sponsor shall be responsible for providing all on-site and off-site infrastructure and public improvements required to provide service for the project area. All such improvements shall be planned and coordinated with the City and other service providers, and shall meet the requirements of the City and such providers.
6. The project sponsor shall have a qualified wildlife biologist to ascertain whether golden eagles are inhabiting the northwestern section of the project area.
7. The project sponsor shall be solely responsible for the improvement of Columbus Parkway from the northern edge of Blue Rock Springs Park to the northerly Ascot Parkway/Columbus Parkway intersection to four lanes as described in the SAP. For the section of Columbus Parkway between I-80 and the northerly Ascot Parkway/Columbus Parkway intersection, the project sponsor shall participate in a benefit district for improvements of six lanes as described in the FSEIR. The City shall determine what type of funding mechanism will be used, which project sponsors and/or property owners shall participate in these improvements, and the timing of these improvements.
8. The project sponsor and the City shall consult with the representatives of Syar Industries in planning the improvements to Columbus Parkway. During such consultation, the following shall be considered:
 - a. The design of intersections on Columbus Parkway;
 - b. The use of a "thru lane," if feasible;
 - c. The timing and synchronization of signals to allow longer flows to ease peak hour conditions; and
 - d. The unimpeded flow of traffic during peak hours.
9. The poles and conduit for all the traffic signals shall be installed initially with the road improvements with the mast arms, heads, controller cabinets and wiring being installed as the City requires.
10. The subdivision and improvement plans for the project shall include the following standards wherever feasible:
 - a. Avoidance of four-way oblique intersections in residential areas;
 - b. Avoidance of driveways on inside curves;
 - c. Sight distance consistent with traffic speed, terrain and alignments;
 - d. Street alignments consistent with the natural topography.
11. Consult with the management of Vallejo Corners to assess the feasibility of access between the two projects.

12. Provide a utility plan to the Public Works Department for review and approval showing joint utility trench, underground vaults, street lights, aboveground boxes including CATV boxes, and any other related facilities.
13. The improvement plans shall include the utilities required to service all public and common facilities. Such utilities shall be constructed at the time of other subdivision improvements.
14. The improvement plans shall show the realignment of Ascot Parkway and area "A" access to provide additional buffering between the three properties at the end of Pinto Court and Ascot Parkway.

E. Prior to issuance of the first building permit:

1. All known and potential slides in areas within or close to proposed development shall be repaired by removing slide debris, installing subdrainage along sound subsoil or bedrock benches, providing catchment areas, and backfilling the excavated areas with engineered and well compacted fill.
2. Buildings and building sites adjacent to unstable slopes shall be designed based on the correction of unstable alluvial and colluvial material at the base of the slopes and shall include features such as catchment trenches, diversion structures and reinforced building walls.
3. The project sponsor shall prepare and submit to the Planning Division for approval a program that will require the use of public art, such as sculptures, monuments, mosaics, murals and fountains, as part of the development of non-residential uses.

F. General conditions:

1. Construction activities shall be limited to daylight hours on weekdays, except when special circumstances warrant additional hours and when approved by the City.
2. The project shall comply with the requirements of the Solano County Mosquito Abatement District during all phases of construction and operation.
3. Liquefiable material at or near the ground surface should be replaced or recompacted before it is used for structural support.
4. The commercial area shall be designed as a single, unified project. The architectural style or "theme" shall create a unique, exciting and quality shopping environment.
5. All landscaped open space areas shall be defensively designed to protect the users of these areas and the surrounding land uses.
6. The project sponsor shall be responsible for the removal and disposal of all siltation that occurs in any off-site retention ponds required for the project.

7. The project shall comply with the requirements of the Greater Vallejo Recreation District for those recreation fees and facilities required by the City and GVRD.
8. The project sponsor shall participate with the City, Greater Vallejo Recreation District, other agencies, groups, and/or area project sponsors in providing a regional trail system in the Sulphur Springs Mountain area.
9. The homeowner association(s) shall participate in a fire prevention program in conjunction with the City's Fire Prevention Division and a crime prevention program in conjunction with the City's Crime Prevention Division.
10. The project sponsor, City and other concerned agencies and groups shall develop and implement a Hazardous Materials Plan for the project to include the mitigation as described on pages 4-209-4-210 of the DSEIR volume if hazardous materials are used within the Business Park.
11. Full disclosure of all Mello-Roos and/or other types of assessment districts within the project area shall be given to all potential purchasers of property within the project area.
12. The project sponsor shall pay all applicable development and plan review fees required by Vallejo Sanitation and Flood Control District.
13. Consideration shall be given to sight distance requirements in the design of grading and landscaping at street intersections and curves.
14. Additional traffic signals within the project area may be required from the project sponsor at intersections not listed in the SAP.
15. All permits from all jurisdictional agencies shall be kept current.
16. All open space shall be accessible by emergency vehicles.
17. The project sponsor shall reserve a site or space within the office or business areas for a child care center. If, after five years from the adoption of the SAP, this site or space is still vacant, then this condition is hereby repealed.
18. Establish with the City and other area project sponsors a Geologic Hazard Abatement District over part or all of the project site if the City requires such a district.
19. If the City does not implement a citywide Transportation System Management ordinance within two years of adoption of the Northgate SAP, then the project sponsor shall develop a TSM program within three years of adoption of the SAP. Such program should include information regarding the availability of the following:
 - a. Linkages between the project and the San Francisco-Vallejo ferry service;

- b. Linkages between the project and BART;
 - c. Bus shuttles within the project area;
 - d. Van pools to employment centers outside the project area.
20. If required by the City, the project sponsor shall participate in a benefit district to improve that section of Admiral Callaghan Lane that is currently two lanes north of Redwood Parkway. Such participation shall be limited to the project's impact on this section of right-of-way.
21. Within three years of the adoption of the SAP, the park and ride lot shall be designed and constructed by the project sponsor. The design shall accommodate transit and bicycle circulation and service. The maintenance of the lot shall be by the City or other agency or group deemed appropriate using funds generated by assessment and/or Mello-Roos district financing.
22. If building construction will take place within the subdivision after the subdivision improvements and the dedication of public rights-of-way for public use are accepted by the City, the public rights-of-way shall not be used for staging building construction activities, including but not limited to storing construction material and equipment. The street and sidewalks must be kept free of construction debris, mud, and obstacles and must remain open to traffic at all times.
23. All progress reports and daily reports generated by the project soils engineer, soils technicians and other firm representatives of the soils engineer regarding site grading must be made available to the City in a timely manner and no later than a week after generation of such report to provide the City with current information relative to the grading operation.
24. Open space parcels owned/controlled by the project sponsor must be free and clear of toxic substances, stock piles of soils, non-approved boulder placements, combustible piles of leaves below 12 feet, measured from the bottom of any tree within 100 feet of structures and fences must be removed, and other open space lands are also subject to the Uniform Fire Code, Division II, "Combustible and Flammable Materials," Section 11.201(a). The existing natural setting of the open spaces must be preserved and maintained except that of approved installations or modifications for landscaping, grading, concrete ditches, fire breaks, trail systems or other approved changes on the contour must preserve the appearance of the natural resource.
25. The illustrative plans submitted as part of the SAP are for discussion purposes only, and shall not be considered as approved site plans. The specific review of site plans for approval shall occur during the subdivision, unit plan and/or use permit application processes.

SPECIFIC PLAN #88-01A

CONDITIONS OF APPROVAL

September 26, 1989

Note: Most of these conditions have been incorporated into the Specific Area Plan included herein.

EXHIBIT A
CONDITIONS OF APPROVAL
AMENDMENT OF SPECIFIC PLAN #88-01

(Adopted by the City Council on September 26, 1989)

(Note: The term "project sponsor" refers to Vallejo Development Company or its successors-in-interest.)

- A. The Northgate Specific Area Plan, as adopted by the City Council on October 25, 1988,, shall be amended as follows:
1. The amount of square footage of nonresidential uses shall not exceed 1,929,000 square feet unless additional environmental review is undertaken. If such review concludes that all traffic and other impacts from an increase in square footage can be mitigated, then the area for nonresidential uses may be increased to a maximum of 2,212,000 square feet, and suggested mitigation measures shall become conditions of the project.
 2. All references to high density residential may be deleted, and the descriptions and standards may be revised to reflect the following:
 - a. The area designated for medium density residential shall be 26.3 acres.
 - b. The maximum number of units shall be 432. This number may be less depending on the configuration of a project or projects approved by the City of Vallejo.
 - c. Any proposed project shall require a Major Use Permit.
 - d. The development standards for any project shall be as described for medium density residential in Specific Plan #88-01, adopted October 25, 1988, and as revised.
 3. The Office Park may be reduced to a minimum of 4.1 acres. The Business Park may be increased to a maximum of 105.9 acres. The following uses shall be allowed in the Business Park subject to Major Use Permits:
 - a. Construction sales and services;
 - b. Repair services;
 - c. Wholesale storage and distribution;
 - d. Light manufacturing and assembly.

In addition, laundry services and horticultural materials and services shall be deleted as allowable uses in the Business Park.

4. The maximum number of low density residential units may be 1,524, although the final number of units may be less depending on the configurations of future approved subdivisions as required by the City of Vallejo. Neighborhood "H" will be permitted as follows: The number of units shall be determined by the City of Vallejo and shall not exceed 48 units. Further, the configuration and total buildable land of the golf course area shall be subject to the acceptance of the Vallejo Golf Club Board of Directors and/or golf course architect, and no portion of Neighborhood "H" shall be immediately adjacent to the proposed new high school. The design for this area shall be subject to review and approval by the City of Vallejo.
5. The extension of Redwood Parkway may be realigned to the north of the City's existing water tank. Neighborhoods "C", "E" and "F" shall be reconfigured to accommodate the realignment.
6. The fire station and the PG&E substation shall be located on the north side of Columbus Parkway next to the park-and-ride. Standards shall be developed to insure all three facilities are visually compatible with each other.
7. The amount of open space within the entire project shall be a minimum of 570 acres.
8. The Blue Rock Springs Golf Course may be extended into the project area with the approval of the City of Vallejo based in input from the Vallejo Golf Club.
9. Fast food restaurants may be allowed in the commercial center when they are in attached buildings within the center. Additionally, and notwithstanding the above, the project sponsor shall be allowed two (2) free standing fast food restaurants subject to Major Use Permits and upon the following conditions:
 - a. One (1) fast food restaurant shall be granted to the project sponsor by the City upon written confirmation from the project sponsor to the City that the Gateway Plaza Retail Center has obtained written commitments for location in the center from the following firms and/or their equivalent:
 - 1) A Fry's grocery store;
 - 2) A Long's drug store;
 - 3) A Home Club home improvement store; and
 - 4) A Ross and/or Marshall's retail clothing store.
 - b. A second fast food restaurant shall be granted to the project sponsor upon the occurrence of any one of the following events:

- 1) Verification from the project sponsor to the City that the project sponsor has secured a retail tenant for 80,000 or more square feet;
- 2) Verification from the project sponsor to the City that the project sponsor has secured two retail tenants of 15,000 square feet or more and one retail tenant of 10,000 square feet or more or a combination of three retail stores totaling a minimum of 40,000 square feet;
- 3) Verification from the project sponsor to the City that the project sponsor has secured a "dinner house" restaurant within the Northgate project of at least 6,000 square feet and including a full bar/cocktail lounge.

The fast food restaurants shall be consistent with the rest of the center in terms of building design, materials, colors, details; landscaping; pedestrian paving surfaces; signage; street and pedestrian furniture. They shall not be allowed to maintain their corporate architecture.

10. "Light Automotive Services" may be added to uses allowed in the commercial center with a Major Use Permit and with the following restrictions:
 - a. Automotive services shall be limited to tune-ups, lubes and oil changes, muffler installations, brake repairs, and wheel alignments.
 - b. Automotive services shall be allowed in only one area of the center in one building. No freestanding pads.
 - c. Automotive services shall be allowed only with the issuance of a Use Permit.
 - d. Automotive services are only allowed when conducted entirely indoors. No overnight storage of vehicles.
 - e. The automotive services building shall be identical to the remainder of the approved center in terms of building design, materials, colors, details; landscaping; pedestrian paving surfaces; signage; and street and pedestrian furniture.
 - f. Automotive services shall not visually impact adjoining uses and/or properties. Service bays shall be screened from adjoining uses and/or properties by the orientation of the building and/or landscaping.
11. One gas station and one car wash are allowed in the commercial center on the east side of Doris Drive subject to a Major Use Permit. The gasoline station and car wash shall be consistent with the approved center in terms of building design, materials, colors, details; landscaping; pedestrian paving surfaces; signage; street and pedestrian furniture.

12. The Phasing Program shall be revised as shown in Table 2 of the staff report with the exception that the improvements of Admiral Callaghan Lane south of Turner Parkway shall remain in Phase I; and shall be revised as required by Condition A.2. of Tentative Map #88-29. With reference to the completion of Admiral Callaghan Lane by the project sponsor, it is agreed and understood that the project sponsor's obligation to complete Admiral Callaghan Lane during Phase I is subject to either:

- a. The project sponsor being able to obtain all necessary permanent and construction easements from the Melvin Cook property; and/or
- b. The City of Vallejo acquiring the necessary permanent and construction easements from the Melvin Cook property by exercising its right of condemnation over said property.

Should either of the above not be completed within the time frame of Phase I, then, and in such event, the project sponsor shall be required to complete Admiral Callaghan Lane during Phase 2.

13. All references to a development agreement shall be deleted.
14. The improvement standards for Columbus Parkway shall be deleted and replaced with the standard that the improvement within the North-gate project shall be compatible with the City's design standards.
15. The 100-foot minimum lot depth in low density residential areas may be changed to 100-foot average lot depth with the restriction that no lot shall be less than 80 feet in depth.
16. All references to Bedford Properties shall be replaced with "Vallejo Development Company or its successors-in-interest."
17. The signage standards shall be revised to reflect the following:
- a. The project sponsor(s) of all the residential projects shall participate in a unified advertising signage program.
 - b. If an auto mall is developed within the Business Park, the City shall consider one off-site sign for the auto mall. The project sponsor shall apply for a Major Use Permit to allow the sign.
 - c. The amount of signage for the commercial, business, and office uses shall not be determined by parcels. Rather, the amount of signage shall be determined by the size of the overall commercial, business, or office project. This is to avoid oversigning when one project may extend over several parcels.
18. The project sponsor shall meet and confer with the City of Vallejo and the Vallejo Sanitation and Flood Control District to resolve the remaining drainage issues within 30 days from Council approval of this Amendment. The project sponsor, the City of Vallejo, and the

Vallejo Sanitation and Flood Control District shall agree upon a short term and a long term solution to the Fairgrounds drainage problem including the determination of the amount of the project sponsor's pro rata share of the costs of said solution within said thirty (30) day period.

19. To complete the required off-site traffic mitigation measures, as set forth in the certified Final EIR, for traffic generated by the project, the applicant shall pay a fee of \$340.00 for all residential units in the project and 15 cents per square foot for commercial office and industrial uses at the time of and as a condition to the issuance of a building permit. Said required payment shall remain in effect unless and until the City of Vallejo adopts a new citywide traffic impact fee applicable to all projects in the City of Vallejo. Upon such enactment, the citywide traffic impact fee will be applicable to the remaining development within this project for which a building permit must be obtained.
- B. Unless specifically affected by the revisions identified in Condition "A" herein above, all conditions of Specific Plan #88-01 imposed on the project on October 25, 1988 shall still be in effect.
- C. Tentative Map #88-29 shall be revised to reflect this amendment to the Specific Area Plan, and the revised vesting tentative map shall be submitted to the Planning Commission for approval.
- D. The project sponsor shall provide copies of the amended SAP to all project consultants, engineers, designers, and successors-in-interest to insure that all projects proposed under the SAP are consistent with the SAP.

PLMS759M

TENTATIVE MAP #88-29A

CONDITIONS OF APPROVAL

October 2, 1989

Note: The conditions from the original vesting tentative map (#88-29) are included within these conditions for the amended map.

TENTATIVE MAP #88-29A

EXHIBIT B

CONDITIONS OF APPROVAL

(amended by the Planning Commission on October 2, 1989)

Notes: The term "project sponsor" is defined as the applicant and/or its successors-in-interest.

The conditions listed below may be required by the applicable department and/or agency to be satisfied at times later than listed below. This change in timing would occur when the department and/or agency determines the condition is not applicable to a certain area of the development site or to a specific project.

An asterisk (*) indicates a new or significantly changed condition imposed on the project because of amendment to the map. All other conditions are as imposed by the Planning Commission on the subdivision on February 6, 1989.

The symbol (M) denotes a mitigation measure recommended by the certified Northgate FSEIR and imposed by the City Council as a condition of project approval, as required by CEQA.

A. Prior to October 26, 1989:

1. To use in reviewing all plans and final maps the project sponsor shall submit a copy of the revised tentative map to both the Planning Division and Public Works Department. This revised map shall show the following:
 - *a. Maximum feasible open space between residential areas "C" and "D", as determined by the Planning Division (subject to appeal to the Planning Commission);
 - *b. Landscape parcels that meet the standards of the SAP between the residential lots and the major streets;
 - *c. Allowance for "entry gates" into each of the residential areas;
 - d. Minimum of 570 acres of open space;
 - e. Access points to open space areas through the residential areas.
- *2. The project sponsor shall meet and confer with the City of Vallejo and the Vallejo Sanitation and Flood Control District to resolve the remaining drainage issues. The project sponsor, the City of Vallejo, and the Vallejo Sanitation and Flood Control District shall agree upon a short term and a long term solution to the Fairgrounds drainage problem including the determination of the amount of the project sponsor's pro rata share of the costs of said solution.

B. Prior to November 15, 1989:

- *1. The project sponsor shall submit plans to the Planning Division, Public Works Department, and Building Division for review and approval for all the retaining walls, screen walls, sound walls, and fencing that will be adjacent to public rights-of-way and public open space. These walls and fencing shall be aesthetically compatible with each other. (Planning, Public Works)
- *2. Submit to the Planning Division one camera-ready copy of the amended SAP as approved by the City Council on September 26, 1989.
3. Submit the revised phasing plan for on-site and off-site street improvements. This phasing plan shall be consistent with Table 2 in the Planning Commission staff report dated September 6, 1989, with the exception that the improvements of Admiral Callaghan Land north of Turner Parkway shall remain in Phase I.

It is agreed and understood that the project sponsor's obligation to complete Admiral Callaghan Lane as part of an assessment district or a benefit district during Phase I is subject to the project sponsor being able to obtain all necessary permanent and construction easements from the Melvin Cook property. Should the project sponsor be unable to obtain the easements given a good faith effort, then the City of Vallejo will acquire the necessary permanent and construction easements from the Melvin Cook property by exercising its right of condemnation over said property. The project sponsor shall commence construction within 60 days of such acquisition.

The phasing plan shall also be revised to reflect the following:

- a. Doris Drive construction shall include installation of a traffic signal at the intersection with Admiral Callaghan Lane.
- b. Relocate existing Turner Parkway from Foothill Drive should be done concurrently with building Turner Parkway. Installation of traffic signal at Turner Parkway and Admiral Callaghan Lane will be required at this time.
- c. Columbus Parkway widening west of Ascot Parkway (north) must be done concurrently with the extension of Ascot Parkway to Columbus Parkway. A traffic signal at this intersection will be required in conjunction with the widening.
- d. Extension and reconstruction of Ascot Parkway to Columbus Parkway shall include the installation of a traffic signal at the intersection of Redwood and Ascot Parkways.
- e. Relocation of Rindler Creek needs to occur with the grading and development of the business park since the creek traverses this site.
- f. Off-site traffic and utility improvements must be installed as development dictates and when demanded by the City.

- g. Construction of the interior road system shall be accomplished in a systematic and orderly manner so that adequate circulation will be provided to each development phase. (SAP, Public Works)

C. Prior to approval of grading permits when required by the Public Works Department and/or VSFCDD:

- (M) 1. Additional studies shall be undertaken and shall include the following:
 - a. Geologic mapping and subsurface investigation with recommendations for mitigating any slope instabilities, landslide conditions, and liquefiable materials.
 - b. Analysis and recommendations for mitigating any groundwater hazards.
 - c. Identification of bedrock orientations and other stability conditions for each cut slope.

These studies shall be reviewed and approved by the Public Works Department prior to authorizing any financing such as the sale of bonds by the City. The project sponsor shall follow all recommendations of such studies, as required by the City. (SAP)

- 2. The draft geotechnical investigation report for the Northgate project (dated October 6, 1988) shall be revised to reflect the following comments:
 - a. The report identifies several wet areas which suggest that a seasonally high ground water level could exist in the valley area (Page 8); but the report does not clearly recommend what measures to follow to mitigate or reduce this wet area. Any such mitigations shall become conditions of this vesting tentative map.
 - b. The report should identify each landslide and each artificial fill area by a reference number on the site geology map. Specific recommendations for slide repair and removal of the artificial fill material could then be referred to each particular slide or fill for the project sponsor to follow, subject to the approval of Public Works.
 - c. Section 5a.iii. General (Page 11) states: "All subdrains should include cleanout risers." The report does not clarify what the purpose is for the cleanout risers. The subdrain system must be a self operating system and should not require maintenance. If there is a need for maintaining the cleanouts, they will not be maintained by the City, VSFCDD or the project sponsor. An alternative subdrain system should be evaluated which requires no maintenance.

Subdrains requiring cleanouts will not be allowed in public rights of way or public open spaces or other public land which

places the obligation for their maintenance and therefore associated liability on the City.

- d. Reference to Section II., Trench Backfill and Appendix C (Page 9). City standard specifications provide for at least 90% compaction of native material except within 2.5 feet of finish grade to not less than 95% relative compaction. These specifications must be followed.
 - e. Design Traffic Index (TI) shall be for 20-year life, not 10-year life, as presented in Table 2 for Areas Underlain by Shale Bed Rock (Page 17). (Public Works)
3. The soils report for this development shall contain a soil corrosivity survey per ANSI/AWWA C105/A21.5-82, Appendix A. (Water)
4. A civil engineering vibration specialist shall conduct a one-time analysis of the attenuation characteristics of the vibrations produced by each type of construction/excavation equipment or activity for each site subjecting existing structures to such vibrations. (SAP)
- (M) 5. A detailed hydraulic and water quality study shall be prepared based upon the storm drainage Master Plan of VSFCO. This study shall include the following:
- a. Recommendations for the realignment of creeks to satisfy the requirements of VSFCO.
 - b. Landscaping and grading standards that will reduce the potential for downstream flooding.
 - c. Standards for drainage patterns and flow paths that will result in infiltration and slower stormwater velocities.
 - d. Analysis of potential water quality impacts to Lake Chabot and measures to mitigate any such impacts.
 - e. Recommendations for the use of permanent sedimentation basins or low velocity flows within the upper reaches of the creeks.
 - f. Standards for the use of fertilizers, pesticides and other hazardous materials.
 - g. Erosion control recommendations.

This study shall be reviewed and approved by the City, VSFCO and all other appropriate agencies and groups, and its recommendations shall be followed by the project sponsor. (SAP)

6. After the hydrology study is made, if it is determined necessary to provide permanent sedimentation basins within the project to collect urban contaminants to prevent degrading of runoff to Lake Chabot, the basins would need periodic cleaning and removal of silt and debris. Should it be determined that the sediments contain levels of hazardous materials, a hazardous material management plan shall

be prepared by the project sponsor and approved by the City and VSFCDD for the removal and disposal of the material. (Public Works)

7. The study described in Condition C.5. shall be the basis for the following monitoring programs:
 - a. Prepare a program to monitor District facilities downstream of this project to and including Lake Chabot. The program shall monitor silts and construction debris reaching the facilities from this project. Impacted pipes shall be cleaned prior to October 1 of each year or more often if necessary to maintain system functions. The program shall be in place prior to acceptance for construction of grading plans except for the grading plans for parcels "F" and "G" as accepted for construction prior to April 1, 1989. (VSFCDD)
 - b. Prepare a program to monitor Lake Chabot water quality and silt levels. Water quality baseline information shall be developed by the developer prior to completion of 500,000 cubic yards of grading. The water quality monitoring program shall be a permanent one. The developer shall implement and operate the monitoring until 60% of the units are occupied. Silt level monitoring shall be a developer responsibility until a point specified in the program. The program shall provide for silt removal by the developer as necessary. The District shall approve the program prior to implementation.

The above plan shall be subject to modification to comply with the Clean Water Act or similar legislation regarding discharges through storm drainage facilities. (VSFCDD)

- (M) 8. Permits from the U.S. Army Corps of Engineers and Department of Fish and Game (and Regional Water Quality Control Board, if required) shall be secured. (SAP)
- (M) 9. The topsoil storage area location(s) shall be approved by the Public Works Department and Planning Division, and shall be located where feasible adjacent to the location(s) where the topsoil will be used in the final landscaping. These locations shall not be adjacent to existing or future residential areas. (SAP)
- (M) 10. The alteration of Rindler Creek shall be reviewed and approved by the Army Corps of Engineers, Department of Fish and Game, Regional Water Quality Control Board, Vallejo Sanitation and Flood Control District and any other permitting agencies. The plans for such alteration shall include the following if recommended by a wildlife biologist and approved by the above agencies:
 - a. Rock lining shall be used only in areas where a high erosion potential exists.
 - b. Check dams, gabion structures and increased creek widths shall be used where appropriate.
 - c. A diversity of native wetland plant species should be used in a three layer development of plant structure.

- d. The Revegetation Manual published by the Alameda Flood Control District shall be used as a design guide. (SAP)
- 11. Develop a trail plan in accordance with City Master Trail Plan to be reviewed by Planning, Public Works Department and GVRD. The trail system shall be shown on all grading and landscaping plans. (Public Works)
- *12. The proposed vesting tentative map is incomplete in that the contours, storm drainage, and sanitary sewers are not consistently shown. (VSFCD)

D. General grading conditions:

- (M) 1. Grading plans for work in the vicinity of PG&E towers shall be reviewed approved by PG&E. (SAP)
- (M) 2. For the review and approval of the City and the Vallejo Sanitation and Flood Control District, a detailed temporary erosion and sedimentation control plan shall be prepared for each construction and operational phase of the project to meet the standards prescribed by ABAG. (SAP)
- (M) 3. The maintenance responsibility for the stormwater system and any sedimentation basins and subdrains during the construction and operation phases shall be established to meet the requirements of the Vallejo Sanitation and Flood Control District. (SAP)
- (M) 4. The project sponsor shall post a performance bond or bonds to satisfy the requirements of the Vallejo Municipal Code. (SAP)
- 5. Provide evidence of written permission from property owners for offsite grading. (VSFCD)
- 6. Lot to lot drainage is not permitted. (VSFCD)
- 7. Drainage from adjacent lands shall not be blocked. (VSFCD)
- 8. Indicate the 100-year flow path on the grading and project plans where it passes through lot areas. Buildings shall be located to avoid these paths. Place a note to this effect on the project plans. (VSFCD)
- 9. No grading shall occur within 100 feet of the center line of Blue Rock Springs Creek unless permitted by GVRD. (GVRD)
- *10. Rough grading of the high school site shall meet the approval of the State Office of Structural Safety and the recommendations of the soils engineer. The grading plans will be prepared by the District's architect, as per the District and Developer agreement. (VCUSD)
- *11. The high school site and residential area "F" indicates grading which could result in piping portions of Blue Rock Springs Creek and its tributaries. A study evaluating the impact of such piping on downstream facilities to Lake Chabot shall be provided if required.

Mitigation measures shall be proposed and implemented for identified adverse impacts. (VSFCD)

12. The grading plans for the elementary school/park site shall meet those plans as adopted by VCUSD and GVRD. This grading is not the responsibility of the project sponsor. (VCUSD, GVRD)
13. During grading operations, the project geologist or soils engineer or their representative and necessary soils testing equipment must be present on site. In the absence of the soils engineer or his or her representative on site, City will shut down the grading operation. (Public Works)
14. All progress reports and daily reports generated by the project soils engineer, soils technicians and other firm representatives of the soils engineer regarding site grading must be submitted to the City in a timely manner, no later than a week after generation; such reports will provide the City with current information relative to the grading operation. (Public Works)
15. If any lot of the subdivision/project is to be regraded after mass grading or interim is completed, a new grading permit is required. A review of the proposed grading by a soils engineer and field testing of the grading will be required. (Public Works)
16. The City Grading Ordinance provides for one and one-half (1-1/2) percent earth swale for finished lot grading. The swale with one percent (1%) slope as shown on "typical plot plan" is unacceptable. (Public Works)
17. In design of grading and landscaping the lines of sight distances must be provided conforming to CalTrans Standards. Installation such as signage, monuments, and above ground utility boxes shall not block the line of sight of traffic. Lines of sight must be preserved to all intersections and driveway access and shall conform to the criteria established by CalTrans. Slopes, fencing or other improvements must be set back as necessary. (SAP, Public Works)
18. The project must comply with the City of Vallejo Flood Damage Control Ordinance. (Public Works)
19. Independent review of geological and soils reports may be required by the City and the project sponsor shall pay for such independent review cost, if required. (Public Works)
- *20. The project sponsor shall submit evidence to the Planning Division that, aside from Rindler Creek, no other areas within the Northgate project are within the jurisdiction of the Corps of Engineers. If such evidence cannot be submitted, and areas are within Corps jurisdiction, then the project sponsor shall be required to secure approval from the Corps. (Planning)

E. Prior to approval of the first final map or improvement plans, whichever occurs first:

1. The project sponsor shall consult with the Vallejo Sanitation and Flood Control District regarding how project-related improvements can lessen any project-related impacts on the District. (SAP)
2. A maintenance program for Rindler Creek shall be prepared, and shall meet the requirements of the City, VSFCD, Corps of Engineers and the Department of Fish and Game. This program shall identify maintenance requirements and responsibilities. (SAP)
- (M) 3. If required by the City, the project sponsor shall provide a repeater site and equipment. (SAP)
- (M) 4. The project sponsor shall have a qualified wildlife biologist ascertain whether golden eagles are inhabiting the northwestern section of the project area. (SAP)
- *5. The Public Works Department and project sponsor shall determine whether rolled curbs shall be used in the project. (Public Works)

F. General conditions for final maps and/or improvement plans:

- (M) 1. A consent agreement shall be secured from PG&E prior to use of transmission rights-of-way. (SAP)
- (M) 2. The project sponsor shall be responsible for providing all the identified on-site and off-site infrastructure and public improvements required to provide service for the project area. All such improvements shall be planned and coordinated with the City and other service providers, and shall meet the requirements of the City and such providers. (SAP)
3. The maximum number of final maps based upon this vesting tentative map that may be filed with the City for approval is twenty-five (25). (Planning)
4. All lots shall be clearly designated with a number or letter. Within the residential neighborhoods "A" through "H", the lots shall be numbered consecutively. (Planning)
- (M) 5. The project sponsor shall be solely responsible for the improvement of Columbus Parkway from the northern edge of Blue Rock Springs Park to the northerly Ascot Parkway/Columbus Parkway intersection to four lanes as described in the SAP. For the section of Columbus Parkway between I-80 and the northerly Ascot Parkway/Columbus Parkway intersection, the project sponsor shall participate in a benefit district for improvements of six lanes as described in the FSEIR. The City shall determine what type of funding mechanism will be used, which project sponsors, adjacent property owners and/or others shall participate in these improvements, and the timing of these improvements. (SAP)
6. The project sponsor and the City shall consult with the representatives of Syar Industries in planning the improvements to Columbus

Parkway. During such consultation, the following shall be considered:

- a. The design of intersections on Columbus Parkway;
 - b. The use of a "thru lane," if feasible;
 - c. The timing and synchronization of signals to allow longer flows to ease peak hour conditions; and
 - d. The unimpeded flow of traffic during peak hours. (SAP)
- (M) 7. The poles and conduit for all the traffic signals shall be installed initially with the road improvements with the mast arms, heads, controller cabinets and wiring being installed as the City requires. (SAP)
8. The subdivision and improvement plans for the project shall include the following standards wherever feasible:
- a. Avoidance of four-way oblique intersections in residential areas;
 - b. Avoidance of driveways on inside curves;
 - c. Sight distance consistent with traffic speed, terrain and alignments;
 - d. Street alignments consistent with the natural topography. (SAP)
9. Provide a utility plan to the Public Works Department for review and approval showing joint utility trench, underground vaults, street lights, aboveground boxes including CATV boxes, and any other related facilities. (SAP)
10. The improvement plans shall include the utilities required to service all public and common facilities. Such utilities shall be constructed at the time of other subdivision improvements. (SAP)
11. The improvement plans shall show the realignment of Ascot Parkway and area "A" access to provide additional buffering between the three properties at the end of Pinto Court and Ascot Parkway, and the installation of a masonry wall for the protection of these lots if required to satisfy City of Vallejo noise standards. (SAP, Public Works)
12. Offsite VSFCDD sanitary and/or storm drainage facilities shall be extended in proper easements to serve this project prior to project plan acceptance or as required by VSFCDD. (VSFCDD)
- *13. Comply with VSFCDD Resolution 4138 regarding improvement of existing offsite storm drain and sanitary sewer facilities. This is a phased project under this resolution. Each submittal for each phase shall include a memo stating the cumulative flows for all submitted phases per affected offsite drainage or sanitary sewer facility. (VSFCDD)

14. The portion of onsite sewage system serving more than one legal parcel shall be a public system. (VSFCD)
15. Rear yard facilities serving more than one lot will not be allowed. (VSFCD)
16. Non-VSFCD facilities serving more than one lot will not be allowed. (VSFCD).
17. Provide all weather access to VSFCD facilities for VSFCD maintenance vehicles. Backing more than 50 feet shall not be required. (VSFCD)
- *18. Measures to reduce the pollutant load of storm waters shall be submitted to VSFCD for review and acceptance. Accepted measures shall be incorporated into the development. The measures used shall be reviewed and updated at two year intervals until acceptance of last lot within the project. The project sponsor shall incorporate the updated measures in unconstructed lots. (VSFCD)
- *19. Design and improvement standards, as they may be updated from time to time by the VSFCD, shall apply to portions of this development which do not have current acceptance to construct. (VSFCD)
20. Use concrete or other materials acceptable to VSFCD downdrains in lieu of pipes on slopes. (VSFCD)
21. Drainage from retaining walls and subdrains shall outfall to open channels where possible instead of storm drain inlets. (VSFCD)
22. Provide a standard cleanout at the right of way/easement line per VSFCD standards. (VSFCD)
23. Locate the sanitary sewer manholes in the lane adjacent to the median in divided streets. It should be no closer than three feet to either the median gutter or lane line. (VSFCD)
24. Provide deed references for existing storm and sanitary sewer easements. (VSFCD)
25. Dedication of storm drain easements shall comply with VSFCD criteria: 15-foot minimum for single pipe; 20-foot minimum for two pipes. (VSFCD)
26. Provide silt disposal areas for any long term silt disposal program approved with this project. (VSFCD)
27. The Rindler Creek cross section shall be adequate to convey District Master Plan flows (100 year) and provide for maintenance of flood carrying capacity. Provide easements and facilities sufficient for this. Move the pipe entering the creek across from the Fire Station Parcel "A" out of the creek bend. (VSFCD)
28. Place the sanitary sewer on the same side of Turner Parkway as the commercial/industrial lots. (VSFCD)

29. The following requested deviations shall not be accepted on any project plans:
 - a. Exceeding manhole standards;
 - b. No all-weather access to VSFCF facilities;
 - c. Curved sanitary sewer alignments. (VSFCF)
30. Where sanitary sewer flow design velocity cannot obtain 2.5 feet per second (fps), then use either 1% minimum slope or 1.5 fps. (VSFCF)
31. Provide drop manholes to reduce line velocities where sanitary sewer flows have flow rates exceed 10 fps. (VSFCF)
32. Place sanitary sewer in concrete casing where it crosses Blue Rock Springs Creek. (VSFCF)
- *33. Final maps shall be submitted separately for the different phases of the project, and shall include the following:
 - a. Improvement plans and bonds that include pumping stations and water tanks that serve the area encompassed by the final maps;
or
 - b. Verification that the water tanks and pumping stations that serve the area encompassed by the final maps have already been constructed or bonded for construction. (Water)
34. A water system master plan shall be prepared and submitted that shall encompass this development and adjacent development in the area and shall be subject to review and approval by the City Water Superintendent. (Water)
- *35. Prior to acceptance of the first phase of development in the 400 pressure zone, the third pump for the Redwood Street pump station shall be installed and operational. The existing pumps and facilities at the same pump station shall be retrofitted to fully comply with the City of Vallejo pumping plant standards dated April 6, 1989. (Water)
- *36. All pumping stations, water tanks, and appurtenances required by this development shall be in full compliance with the City of Vallejo pumping plant and reservoir standards dated April 6, 1989 and May 23, 1989 respectively. (Water)
37. Water system improvement plans shall be submitted to the Water Superintendent for review and approval, and shall contain at least:
 - a. Location and size of fire sprinkler service connection(s) where required;
 - b. Location and size of domestic service connection(s);
 - c. Location and size of irrigation service connection(s);

- d. Location of fire hydrants;
 - e. Location of structures with respect to existing public water system improvements, such as mains, meters, etc.;
 - f. Location and size of new water mains;
 - g. Location and size of any off-site water system improvements. (Water)
38. Easements shall be provided for all water system improvements installed outside the public right of way:
- a. 15 feet wide for water mains.
 - b. 10 feet wide for fire hydrants, water meters, backflow prevention devices, double detector check valves, etc. (Water)
39. Water tank installations are the responsibility of the project sponsor, and shall include, but are not limited to: site preparation, tank construction, water system piping, telemetry, security fencing, paved access road, gates, landscaping, drainage, and utilities. (Water)
40. The water system improvements shall be consistent with the Vallejo Water System Master Plan, 1985, prepared by Kennedy/Jenks Engineers. (Water)
41. The water system inside the development shall be public. (Water)
42. Each house/building shall be metered separately. (Water)
43. Pump station installations are the responsibility of the project sponsor, and shall include, but are not limited to: site preparation, pumps, drivers, appurtenant mechanical and electrical equipment, the structure housing the pumps, water system piping, telemetry, security fencing, paved access road, gates, landscaping, drainage, and utilities. (Water)
44. Hydraulic calculations shall be submitted to the Water Superintendent demonstrating that the fire flow requirements are satisfied. (Water)
45. Fire hydrant placement and fire sprinkler system installation, if any, shall meet the requirements of the Fire Department. (Water)
46. Fire flow and pressure requirements of the Fire Department shall be satisfied. Fire flow at no less than 25 psi residual pressure shall be available within 1000 feet of any structure. One half of the fire flow shall be available within 300 feet of any structure. For single family residential units, the fire flow is 1500 gpm. For other developments, see the Vallejo Water System Master Plan, 1985, prepared by Kennedy/Jenks Engineers. (Water)
- *47. Participate with other affected property owners as required by the City in the cost of design, construction and easement acquisition

- for a new 24-inch pipeline, pump stations, and appurtenances from Fleming Hill up to the limits of this development. This participation cost shall be due and payable to the City upon connection of this development to said 24-inch pipeline. (Water)
48. Water tanks and associated pump stations shall be constructed to the sizes and capacities meeting ultimate development demands at the time they are required by the first development. (Water)
49. Provide needed access easements to existing water system facilities encompassed by this development. (Water)
- *50. Upon written request of the City, the project sponsor shall dedicate the needed parcel and easements for 6.0 million gallon (292 zone) reservoir and appurtenances including access road. (Water)
- *51. The proposed grading south of the existing Hunter Ranch access road shall not be allowed closer than 10 feet to the existing edge of pavement of access road. Related grading work and improvement work along Redwood Parkway extension must be reviewed and approved by the City. Moreover, the relocation of the existing inlet/outlet pipeline, paved access road, security gate and fencing shall have to be completed prior to any work on Redwood Parkway extension and other development in the vicinity of Hunter Ranch tank. New easements shall be dedicated to the City regarding this relocation of water facilities and access road. Plans for this relocation shall likewise be subject to review and approval by the Water Superintendent. (Water)
- *52. The proposed waterline easement for the second inlet/outlet pipeline for the Hunter Ranch tanks may not be acceptable to the City. The project sponsor shall dedicate the easement for the second inlet/outlet pipeline in an alignment acceptable to the City as reviewed and approved by the Water Superintendent. (Water)
- *53. If required by the City, the sponsor shall negotiate the relocation of the existing Columbus Parkway raw water tank, relocate and realign the pipelines leading to and from the existing and now raw water tank including appurtenances. Pad elevation of the new raw water tank shall be at a maximum elevation of 420 feet. If required, the project sponsor shall provide new paved access easement with security gate/s, perimeter fencing and landscaping as required. Plans for the new raw water tank, pipelines, appurtenances, access road easements, fencing, landscaping and all other related work shall be subject to review and approval by the Water Superintendent. (Water)
54. Reserve a public utility easement (P.U.E.) for underground telephone facilities in the following locations or alternative locations as approved by Pacific Bell:
- a. 20' x 20' at the northwest corner of parcel "B" at the intersection of Columbus Parkway and Admiral Callaghan Lane.
 - b. 20' x 20' directly east of the intersection of Ascot Parkway and the access road to area "A."

- c. 20' x 40' at the northeast corner of Ascot Parkway and Redwood Parkway. (Pacific Bell)
55. Exterior lighting shall be high pressure sodium vapor (HPS) type or other type approved by Crime Prevention, all pole mounted fixtures shall be a minimum of 8' in height from ground to the base of fixture. Pole shall have a top mounted reflective housing capable of reflecting lighting downward. (Police)
56. The title sheets of all final maps shall reflect the current owners. (Planning)
57. The zoning of the area covered by the vesting tentative map is MUPD, Mixed Use Planned Development. The title sheets of all final maps shall reflect this zoning. (Planning)
58. The boundaries of the open space shall be monumented. (Public Works)
59. Provide access control to open space parcels to be approved by the Public Works Department and Planning Division. (Public Works)
60. Provide access road to PG&E towers and other utility facilities. The locations and improvement of the access roads shall be approved by the Public Works Department, Planning Division and concerned utility company. (Public Works)
61. The Fire Department shall have dedicated access to all open space areas. (Fire)
62. Establish a Homeowners Association or maintenance district agreement for operation and maintenance of private open space, landscaping, irrigation system, drainage ditches, fences, masonry walls, private streets, private street light system, water quality measures, and other private facilities subject to the approval of the Planning Division, Public Works Director, and the City Attorney. The Covenants, Conditions and Restrictions of all deeds issued within the subdivision shall contain provisions requiring participation in the said Homeowners Association. The Homeowners Association may accept the subdivision prior to acceptance by the City if required by the City. (Public Works)
63. Submit to the Public Works Department for review and approval a street signing and striping plan. This plan shall comply with CalTrans and City of Vallejo standards. (Public Works)
64. All new project traffic signals shall be interconnected by wire and tied to a master controller. (Public Works)
65. Subdivision lighting location and design within the public right-of-way shall be reviewed and approved by the Public Works Department and Planning Division. (Public Works)
66. All permanent existing and future utility lines serving the project shall be undergrounded with the exception of the permanent PG&E towers. (Public Works)

67. For cul-de-sac bulbs with 44-foot right-of-way radius, a detail design shall be submitted showing how all utilities, driveways and street trees shall be accommodated. (Public Works)
- *68. Provide a complete revised island channelized free right turn for all four approaches at the intersection of Redwood Parkway and Ascot Parkway. (Public Works)
69. Modify medians on existing streets to provide left turn to new streets and lengthen left turn pockets on existing streets as determined by the Public Works Department. (Public Works)
70. Interim roads for the business, office and industrial park parcels shall be aligned to provide for circulation of traffic and for looping of utility mains or as directed by Public Works. (Public Works)
71. Provide bus turnouts needed to serve the Northgate project. The location, type and design of such turnouts shall be submitted for review and approval by the Public Works Department, Planning Division, and Vallejo Transit Lines. (Public Works)
72. Setback the development to provide sufficient right-of-way for improvements of Admiral Callaghan Lane at Columbus Parkway as defined in the traffic study (shown on Detail 1, Figure IV-1 of DKS Traffic Report). (Public Works)
73. Median treatment at left turn pockets and other non-planting areas shall be bomanite or other material as approved by the City. (Public Works)
74. Entrances to any private project must be standard driveway approaches unless deviation is permitted by Public Works. (Public Works)
75. Design of street section for all public streets shall be for 20 years life. (Public Works)
76. Provide parking bays on all arterials for emergency parking. (Public Works)
77. Obtain street excavation permit from the Department of Public Works for performing any work within City streets. (Public Works)
78. Vertical and horizontal alignment of all streets shall conform to City of Vallejo Standards. (Public Works)
79. All pads adjacent to open space down slope shall have a minimum of 3 feet property line setback from the top of slope to provide a level bench for fence maintenance and to accommodate potential erosion. (Public Works)
- *80. Retaining walls of any height on property line, in right-of-way or retaining public property or right-of-way must be solid wall (i.e., concrete, block or crib wall) and must be engineered. Retaining walls must be reviewed by the Public Works Department and Building

Division. Building permit will be required. Retaining wall between two lots shall be on down hill lot. (Public Works)

- *81. The City code provides for maximum 14% slope on residential drive-ways. The 2:1 slope as shown on "typical front and rear yard set back detail" may not achieve the maximum driveway grade requirement. However, the SAP requires a maximum of 10% slope. The project sponsor shall discuss this requirement with Public Works and Planning, and change this detail if required. (Public Works, Planning)
- 82. This project shall be subject to changes or updates to the City of Vallejo public improvement standards prior to acceptance of plans for plan check should such changes or updating be applicable city-wide if required by the City. (Public Works)
- 83. Plans submitted to the Public Works Department for plan check shall be a completed set including all supporting documentation, calculations and pertinent reports. Plan set shall include but not be limited to final maps, grading and erosion control plans, improvement plans, joint trench utility and street light plans and landscape, irrigation and fencing plans. (Public Works)
- *84. Provide a standard cul-de-sac at the end of the access road to the park-and-ride lot and the fire station or adequate turn-around space within the facilities. (Public Works)
- *85. The center line of through streets at any intersection must have the same alignment (no offset) at those locations where required by Public Works after discussions with the project sponsor. A transition shall be provided in the narrower street when the street width changes after the section. (Public Works)
- *86. Subdrains shallower than 15 feet that are crossing single family lots must be disclosed in the deed to home buyers. (Public Works)
- *87. All street names shall be approved by the Fire Department and the Public Works Department. One of the street names within the Northgate project shall be "Topley." (Public Works)
- *88. Street name signage shall be in conformance with City standards. In addition, the project shall provide street name signs on all medians for all major four-lane streets. Typically, the median street name sign is a CalTrans G-7 sign posted at both ends. (Public Works)

G. Prior to approval of the final map(s) and/or improvement plans for Parcels "B", "C", "D", "E", "F", and "G":

- 1. Dedicate right-of-way for the widening of Admiral Callaghan Lane at Columbus Parkway and reconstruct Admiral Callaghan Lane at Columbus Parkway to comply with requirements of the traffic study if required by the City. (Public Works)
- 2. Dedicate right of way widening of for Columbus Parkway if determined necessary by the City. (Public Works)

3. Access to Business Park site from Columbus Parkway, Ascot Parkway and Turner Parkway must be approved by the City. (Public Works)
4. Parcel "D" shall have access from Admiral Callaghan Lane as approved by Public Works. (Public Works)
- *5. As a result of DKS Associates' traffic study, the traffic signal installation at the intersection of the major driveway and Doris Drive (central driveway on Doris Drive) is warranted. This signal must be installed prior to the opening of the west driveway or extending Turner Parkway to carry through traffic. (Public Works)
- *6. The nature of the basin in Parcel "G" is unclear. Submit details to Public Works and VSFCDD for review. (Public Works, VSFCDD)
7. Treatment along realigned Rindler Creek Channel and the bank along the creek shall be subject to approval by the City and agencies having jurisdiction on the creek. (Public Works)
8. Access to park and ride lot on Columbus Parkway shall be provided across from Ascot Parkway and controlled by the traffic signal at the intersection of Ascot and Columbus Parkways. (Public Works)

H. Prior to approval of the final map(s) for the low density residential areas:

1. On the final map(s), the drawing showing typical setbacks shall include the following note: "Sideyard setbacks are shown as the minimum standards only. The final setbacks shall be determined by the City based upon the final housing product type." (Planning)
2. All lot dimensions of low density residential lots shall meet or exceed the minimum standards contained in the SAP. (Planning)
3. The project sponsor shall confer with the U.S. Postal Service authorities to determine locations of cluster mailboxes and collection boxes. The final map(s) shall show easements or other mapped provisions for the placement of centralized mail delivery units and/or collection boxes. Specific locations for such units shall be approved by the Planning Division, Public Works Department and the U.S. Postal Service. (U.S. Postal Service)
4. Provision may be made for more street openings within the residential areas "A", "B" and "C" for better traffic mobility. (Public Works)
- *5. The property lines for lots backing to an uphill open space slope shall extend to a level bench. (Public Works)

I. Prior to approval of the final map or improvement plans for residential area "A", whichever occurs first:

- *1. Connect sanitary sewer in "A5" Street at lot 62 to Turner Parkway sewer. (VSFCDD)

2. The existing Turner Parkway serving residential area "A" shall be 36 feet curb to curb with minimum 2.5 feet property line setback from back of sidewalk and 40 feet curb to curb for section between first intersection ("A" Street) to Ascot Parkway with 5 feet property line setback from back of sidewalk. The transition from 40 feet to 36 feet width must be approved by the City. Overlay of the existing half of the street may be required if justified by the City because of deterioration of existing street due to construction activity and/or utility trenching. (Public Works)
- J. Prior to acceptance of residential areas "B", "C", or "D", whichever occurs first:
1. Parcel "I" shall be dedicated to the City, provided that the parcel is free from unrepaired landslides, debris, toxic waste, and complies with Condition EE.28. (Planning)
 2. The project sponsor shall construct a trail/access road to meet the requirements of the City, GVRD and VSFCDC. (Planning, Public Works, Fire, GVRD, VSFCDC)
- K. Prior to approval of the final map or improvement plans for residential area "B", whichever occurs first:
- *1. Revise the cross drainage of lots 21 through 34. (VSFCDC)
 2. Provide joint use agreements for facilities crossing the PG&E easement. (VSFCDC)
 3. Show the 100-year flow path passing through lots 72 and 73. (VSFCDC)
 4. Designate the open space parcel between Ascot Parkway and residential area "B" with a letter "M". (Planning)
 5. Drainage and treatment of slope above "I" Court needs further evaluation. (Public Works)
 - *6. Redesign "E" Street such that the intersection with Ascot Parkway provides for a perpendicular tangent of a minimum 22 feet from the curb line on Ascot Parkway. (Public Works)
 - *7. "G" Street between Ascot Parkway and "E" Street shall have a 60 foot right of way and 40 feet curb to curb. Raised medians within residential streets are not acceptable. (Public Works)
- L. Prior to acceptance of residential area "B":
1. Parcel "M" shall be dedicated to the City provided that the parcel is free from unrepaired landslides, debris, toxic waste, and complies with Condition EE.28. (Public Works)
- M. Prior to approval of the final maps or improvement plans for residential area "C", whichever occurs first:
- *1. Revise the grade of "P" Court to eliminate 100-year drainage out the rear of the cul-de-sac. Slope grade towards "B" Circle. (VSFCDC)

- N. Prior to approval of the final map or improvement plans for residential area "D", whichever occurs first:
- *1. Show the 100-year flow path from "T" Court to Parcel "B". (VSFCD)
- O. Prior to approval of the final map or improvement plans for residential area "E", whichever occurs first:
- *1. "M" Street meets the characteristics of a residential collector street and must be designed as a 40 foot curb to curb street with 50 foot right of way from "E" Street to Ascot Parkway. (Public Works)
- P. Prior to acceptance of residential area "E":
- *1. Parcel "J" shall be dedicated to the City provided that the parcel is free from unrepaired landslides, debris, toxic waste, and complies with Condition EE.28. (Public Works)
 - 2. The project sponsor shall construct a trail/access road to meet the requirements of the City, GVRD, VSFCD and PG&E. (Planning, Public Works, Fire, GVRD, VSFCD)
- Q. Prior to acceptance of residential areas "E" and "F", whichever occurs first:
- 1. Parcel "K" shall be dedicated to the City provided that the parcel is free from unrepaired landslides, debris, toxic waste and complies with Condition EE.28. (Public Works)
 - 2. The project sponsor shall construct a trail/access road to meet the requirements of the City, GVRD and VSFCD. (Planning, Public Works, Fire, GVRD, VSFCD)
 - 3. The storm drain ditches on open space parcel "K" need further evaluation on adequacy and how they will be tied to storm drain system. The City and VSFCD shall review and approve their design. (Public Works)
- R. Prior to approval of the final map or improvement plans for residential area "F", whichever occurs first:
- 1. Storm drain pipe south of Columbus Parkway discharging to golf course parcel is not acceptable. The City and VSFCD shall review and approve a new design. (Public Works)
 - 2. Those residential lots that are adjacent to the golf course shall include landscaping, fencing or other methods deemed appropriate by the City to reduce the impacts of golf balls on the residents. In addition, the CC&Rs shall include language that will hold the City and Vallejo Golf Club harmless from claims for golf-related residential property damage. (Public Works)
 - 3. The project sponsor shall prepare an amendment to this vesting tentative map reflecting the expansion and consistent with the requirements for such amendments in the Subdivision Ordinance. (Planning)

4. The revisions to residential area "F" shall demonstrate that the golf course is an integral part of the development, and that the design of the lots adjacent to the golf course takes advantage of their desirable location. (Planning)
 5. The revisions shall address and mitigate the potential visual impact of having a "wall" of identical units on identical lots lining the edge of the golf course. Possible mitigation measures include, but are not limited to, varying widths of lots, varying setbacks, elevation changes, street alignments and the use of adequate view corridors. In addition, the residents of area "F" should have visual access to the golf course through the use of view corridors and/or mini parks. (Planning)
 6. The density of residential area "F" shall not exceed 4.8 units per acre. (Planning)
 - *7. Map does not provide pad elevations for Neighborhood "F". Department of Public Works' comment based on limited information is that the project must provide drainage facilities behind lots with an up slope to intercept runoff from the slopes. (Public Works)
- S. Prior to approval of the final map or improvement plans for residential area "G":
- *1. Combine storm drainage outfalls. Eliminate adverse aligned outfalls. (VSFCD)
 2. Parcel "H" shall be dedicated to the City as part of the Blue Rock Springs Corridor provided that the parcel is free from unrepaired landslides, debris, toxic waste and complies with Condition EE.28. (Planning, GVRD)
- T. Prior to approval of the final map or improvement plans for residential area "H":
- *1. Neighborhood "H" will be permitted as follows: The number of units shall be determined by the City of Vallejo and shall not exceed 48 units. Further, the configuration and total buildable land of the golf course area shall be subject to the acceptance of the Vallejo Golf Club Board of Directors and/or golf course architect, and no portion of Neighborhood "H" shall be immediately adjacent to the proposed new high school. The design for this area shall be subject to review and approval by the City of Vallejo. (SAP)
 - *2. The plat submitted for Neighborhood "H" does not show contour elevations and proposed pad elevations. Generally drainage facilities must be installed behind the proposed lots to intercept run off from slopes. (Public Works)
 - *3. The proposed cul de sac is 800 feet long. City standard provides for a maximum 500 foot long cul de sac street. The final configuration shall be determined by Public Works and Planning based upon discussions with the project sponsor. (Public Works)

- *4. Access to the proposed Neighborhood "H" from Redwood Parkway will be right-in, right-out only, if the access to "H" and Neighborhood "C" do not line up. (Public Works)

U. Prior to approval of the final map or improvement plans for parcel "L", whichever occurs first:

1. An easement shall be delineated between Columbus Parkway and Parcel "L". (Planning)

V. Prior to the approval of any landscaping plans:

1. Provide with the subdivision improvement plan submittal a landscape and irrigation plan which is in conformance with the City Landscape Guidelines for review by the Public Works Department and the Planning Division. (Public Works)
2. When landscaping is required for commercial, industrial or multifamily projects, the project sponsor shall submit plans to the City for review. The landscape architect for the project must perform a complete and thorough field review of the landscape, irrigation and planting during installation and provide the City in writing that all landscaping and irrigation within the project is in full compliance with the City ordinances and guidelines and the approved planting and irrigation plans. (Public Works)
3. All landscaping shall meet the following general requirements:
 - a. Landscaping and irrigation plans shall be formulated by a qualified slope control specialist who is a professional landscape architect or other person experienced in erosion control work and revegetation. (Public Works, Planning)
 - b. All banks or recontoured slopes greater than twelve (12) feet vertical height shall be landscaped both for long term erosion, fire and weed control as well as aesthetic revegetation considerations. (Public Works, Planning)
 - c. Slope landscaping shall include: trees planted equivalent to one per 400 square feet and/or two (2) five gallon shrubs per 400 square feet; and ground covers planted at sufficient density to give total slope coverage within six (6) months. (Public Works, Planning)
 - d. Slopes facing public or utility rights-of-way, public open space, or at the perimeter of the subdivision shall have the greater proportion of trees and large material; back-to-back interior slopes between house lots will have a greater proportion of shrubs and ground covers. (Public Works, Planning)
 - e. Required planting on slopes shall be fire retardant, especially at the top and toe of the slope. (Public Works, Planning)
 - f. Special consideration shall be given to landscaping natural slopes to create the effect of an open woodland. (Public Works, Planning)

- g. Landscaping plans shall include the use of security plantings where developed areas border open space areas. (Police)
- h. All landscape material around doorways, walkways, breezeways and windows and sliding glass doors shall be low growth. Trees shall not be planted close to buildings or fences. (Police)
- i. The CC&R's shall include a provision that the property owner of each low density residential unit shall install front yard landscaping within six months of the close of escrow. (Planning)
- j. The developer/owner must take the following steps to ensure that landscaping is undertaken with efficient water use in mind:
 - 1) Design and install at least one model home landscape out of each group of model homes and/or sales office in the Northgate development as a Xeriscape type demonstration garden. Signs or cards shall be displayed to alert people touring the model homes as to the special nature of the landscaping.
 - 2) For all areas of high or medium density residential development where landscaping is required by the City, design and install Xeriscape type landscaping in amount proportional to those exhibited by the model homes and/or sales offices which serve the area. As an example, if one model home in a group of four model homes contains a Xeriscape demonstration garden then one quarter of the required landscapes (on a square foot basis) in the area shall be Xeriscape type landscapes.
 - 3) Provide designs for all commercial development areas acceptable to the City which show considerable restraint in lawn area, sophistication in irrigation service, and drought resistance in specimen selection.
 - 4) Provide designs for all public use areas acceptable to the City which show considerable restraint in lawn area, sophistication in irrigation service, and drought resistance in specimen selection. Additionally, designs for public use areas must comply with the City Landscaping Guidelines for Hillside Developments. Drip irrigation and bubbler type heads shall be employed on slopes and at roadsides to prevent runoff of water.
 - 5) If water consumption analyses are produced by the landscape architect, provide the City with copies. The analyses should adjust for the local evapotranspiration (ET_o) rate. (Public Works)
- 4. The project sponsor is required to install street trees in accordance with the Vallejo Municipal Code, City approved landscape planting plans, and the following requirements:

- a. The project sponsor's sales office shall advise in writing and provide a plan for the prospective new home buyer to sign, acknowledging the street tree planting plan for the subdivision unit. (Public Works, Planning)
 - b. The CC&R's shall describe the homeowner's responsibility to maintain the street tree as installed within the planting easement. (Public Works, Planning)
 - c. Street trees shall be planted prior to occupancy release by the construction inspector. Conditional releases may be considered by the Department of Public Works. With each conditional release, the new homeowner shall sign a document provided by the project sponsor acknowledging the installation of required street tree. (Public Works, Planning)
 - d. The Public Works Department will prepare and provide a street tree punch list and the project sponsor shall correct all deficiencies before City Council approval of the subdivision unit. (Public Works, Planning)
 - e. The project sponsor shall, at its cost, provide the new homeowner with a copy of the City's Homeowners Guide for Beautiful, Safe and Healthy Trees. (Public Works, Planning)
 - f. The project sponsor is ultimately responsible for the street trees until the end of the one year maintenance period on public improvements. After acceptance of the subdivision, at the end of the one year maintenance period, the project sponsor shall replace any trees determined to be dead, or missing. (Public Works, Planning)
5. All landscaping on slopes contained within low density residential lots shall meet the following requirements:
- a. The landscape planting plan for the rear and side lot slopes of private lots shall be reviewed and approved by the Public Works Department. (Public Works, Planning)
 - b. The irrigation plan for the rear and side lot slopes of private lots shall be reviewed and approved by the Public Works Department. The source of water and meter shall be from the house on the lot. (Public Works, Planning)
 - c. The Public Works Department shall inspect the installation and review the maintenance program for the establishment of the intended landscape design. The project sponsor shall be responsible for maintaining the irrigation and landscape system's for a period of six months, following the occupancy and/or the installation of the irrigation and planting. At the end of the six months maintenance period the Public Works staff shall evaluate each lot to assure that at least 75 percent of the intended vegetation has been established on the slope. The project sponsor shall be notified in writing if certain lots are deficient in plant coverage, direct the project sponsor to

replant and provide an additional 90-day maintenance period.
(Public Works, Planning)

- d. The CC&R's for the approved project shall include provisions that the project sponsor is provided legal access to the new yards to maintain the approved and installed landscaping and irrigation systems. The time period for such access will be for the required six (6) months plus any additional time to correct unsatisfactory landscaping and irrigation after the initial six months. The homeowner shall be made aware of the Hillside Ordinance recognizing their responsibilities to help plant establishment including adequate watering. (Public Works, Planning)
- e. The Building Division shall inspect the required backflow device at the house connection. A permit from the Building Division is required. (Public Works, Planning)
- f. The required installation of the landscaping and the irrigation system shall be completed before the occupancy of the unit can be released. (Public Works, Planning)

6. Within those areas that are included in a maintenance assessment district, all landscaping shall meet the following requirements:

- a. Areas to be included in a maintenance assessment district shall be determined by the Public Works Department and the Planning Division according to the following criteria:

- 1) Large, publicly visible slopes and/or hilltops at perimeter of subdivision.
- 2) Steep slopes abutting existing property.
- 3) Cut or fill slopes greater than 15 feet vertical distance. Developer must provide level maintenance access to either the top or toe of any large, isolated banks at time of preliminary grading plans.

Easement areas across five or more horizontally contiguous lots shall have maintenance access at each end of the easement.

Easement shall run through central portion of slope face encompassing all areas from the top of the slope to twenty-five feet from the toe of the slope. On interior slopes, a minimum of a six-foot bench on the slope shall be provided adjacent to the property line between the privately and publicly maintained slope. (Public Works, Planning)

- b. Easement areas shall be irrigated with an approved irrigation system reviewed and approved by the Public Works Department. (Public Works, Planning)

- c. The planting plan shall be reviewed and approved by the Public Works Department. All street trees shall conform to the approved City street tree list. Attention must be given to maintaining traffic sight distances at intersections. (Public Works, Planning)
 - d. The Public Works Department shall inspect the installation and maintenance of the landscaping and irrigation system. The developer shall be responsible for maintaining the approved and installed landscaping and irrigation system from the time of installation to one year after City Council acceptance of subdivision to assure establishment of the landscape materials. (Public Works, Planning)
 - e. After one year of project sponsor maintenance, following acceptance of the subdivision by the City of Vallejo, the Landscape Maintenance District administered through the Department of Public Works, will be responsible for maintenance of landscaping within the Landscape Maintenance District. The project sponsor at this time shall provide in writing to the Public Works Department that all irrigation meter accounts have been reassigned for billing purposes to the respective landscape maintenance districts. (Public Works, Planning)
 - f. The CC&R's for the proposed project shall include provisions that the homeowner participate in the Landscape Maintenance Assessment District and that the homeowner protect the required landscape and irrigation system. (Public Works, Planning)
 - g. Water meters for irrigation systems to be maintained by Landscape Maintenance Districts shall be furnished and installed by the City upon payment of applicable fees and charges by the project sponsor. (Water)
7. A fencing program shall accompany the landscape plans, and shall be approved by the Public Works Department and Planning Division. This program shall establish standards for the following:
- a. Fence separating project area from non-project lands;
 - b. Fence separating public lands from all privately owned lands;
 - c. Fencing on private property.

Fencing along the top of slopes of open space parcels shall be set back three feet from the top of slope to accommodate potential erosion of the slope. (Public Works)

W. Prior to transfer of title on any parcel:

- 1. The project sponsor shall create a landscape maintenance assessment district or Mello-Roos District, the boundary of which shall encompass the entire Northgate development to fund the maintenance of public open space including, but not limited to landscaping, irrigation, trails, ditches, walls, fences, water quality measures, administration costs and other related facilities. The district shall

be created such that adjoining properties can be annexed to the same district. The LMD shall be created prior to the transfer of title on any parcel. If the LMD cannot be set up prior to transfer of title of any parcel, then the project sponsor shall include in any sales agreement a provision that the buyer shall agree to participate in the LMD. (Public Works, Planning)

2. The project sponsor and/or the sales representatives shall make full disclosure of the scope of the Northgate project to all potential buyers of property within the project area. This disclosure shall include, but not be limited to, the following:
 - a. All land uses within the project area;
 - b. The intensity and location of uses within the non-residential areas;
 - c. The number, density and location of residential units;
 - d. Street improvements;
 - e. Disposition of open space.
3. Full disclosure of all Mello-Roos and/or other types of assessment districts within the project area shall be given to all potential purchasers of property within the project area. (SAP)

X. Prior to approval of Planned Development Unit Plan permits for each low density residential area:

1. Each low density residential area shall be reviewed and approved by the Planning Division through the Planned Development Unit Plan process. The plans shall show each area as a distinct neighborhood with a style and character of its own while still being consistent with the standards contained in the SAP. This distinct character and style shall be demonstrated through the following:
 - a. Architecture;
 - b. Neighborhood entries;
 - c. Signage;
 - d. Front yard landscaping;
 - e. Street trees;
 - f. Lighting;
 - g. Fencing; and
 - h. Other details as required by the Planning Division. (Planning)
2. Non-pervious surfaces shall cover no more than 50% of the front yard setback area of each low density residential unit. (Planning)

3. Single family homes shall be provided with illuminated house numbers, a minimum of 4" in height. (Police)

Y. Prior to approval of the Major Use Permit for the medium density residential areas:

1. The medium density residential area shall not be developed until the tentative map (if the units will be sold) and Major Use Permit applications are approved by the City. If a subdivision is proposed, then the applications for the map and the Use Permit shall be submitted concurrently to the Planning Division. (Planning)

Z. Prior to obtaining connection permits from the Vallejo Sanitation and Flood Control District:

1. Record a permanent right-of-entry document prior to obtaining connection permits from the District. The document shall provide that the District shall have the right to enter upon the property for the purpose of testing the private portion of sewer lateral exterior to any building in accordance with District ordinances regarding the testing and repair of sanitary sewer laterals. It shall also state that the property may be subject to lien in accordance with District ordinances and regulations in the event the owner fails to correct discovered deficiencies. Place a note reflecting this requirement of the project plans. (VSFCD)

2. Pay all applicable connection fees required by VSFCD. (VSFCD)

AA. Prior to issuance of the first building permit:

- (M) 1. All known and potential slides in areas within or close to proposed development shall be repaired by removing slide debris, installing subdrainage along sound subsoil or bedrock benches, providing catchment areas, and backfilling the excavated areas with engineered and well compacted fill. (SAP)
- (M) 2. Buildings and building sites adjacent to unstable slopes shall be designed based on the correction of unstable alluvial and colluvial material at the base of the slopes and shall include features such as catchment trenches, diversion structures and reinforced building walls. (SAP)

BB. General conditions for building permits:

1. Prior to obtaining building permits for multifamily residential project with private streets, the project sponsor may file with the City of Vallejo, pursuant to the State of California Vehicle Code, a petition requesting that the provisions of said Vehicle Code be applied to private streets in the subdivision and post and maintain signs at the entrances to the subdivision giving notice of enforcement of said Vehicle Code. (Public Works)
2. Between October 15th and April 16th of every year, paved access shall be provided to every lot under construction and built-out. (Fire)

- *3. To complete the required off-site traffic mitigation measures, as set forth in the certified Final EIR, for traffic generated by the project, the applicant shall pay a fee of \$340.00 for all residential units in the project and 15 cents per square foot for commercial office and industrial uses at the time of and as a condition to the issuance of a building permit. Said required payment shall remain in effect unless and until the City of Vallejo adopts a new citywide traffic impact fee applicable to all projects in the City of Vallejo. Upon such enactment, the citywide traffic impact fee will be applicable to the remaining development within this project for which a building permit must be obtained. (SAP)

CC. During construction:

- (M) 1. Construction activities shall be limited to daylight hours on weekdays, except when special circumstances warrant additional hours or shorter hours and when approved or required by the City. (SAP, Building)
2. If building construction will take place within the subdivision after the subdivision improvements and the dedication of public rights-of-way for public use are accepted by the City, the public rights-of-way shall not be used for staging building construction activities, including but not limited to storing construction material and equipment. The street and sidewalks must be kept free of construction debris, mud, and obstacles and must remain open to traffic at all times. Streets serving occupied units shall be subjected to the same conditions. (SAP, Public Works)
3. All construction material and equipment shall be fenced and illuminated during nonbusiness hours. (Police)
4. The project sponsor or general contractor shall provide Crime Prevention with the names and phone numbers of at least two (2) persons who would be able to respond to the job site after regular business hours. This list shall be updated by the project sponsor or general contractor as needed. (Police)

DD. Prior to occupancy of structures:

1. Crime Prevention shall inspect all structures. (Police)
2. Every building constructed shall have a dependable water supply equal to 50% of the required fire flow available within 300 feet driving distance, and 100% of the required fire flow shall be available at hydrants located within 1,000 feet. (Fire)
3. All structures shall have spark arresters. (Fire)

EE. General project conditions:

1. All development within the project area may be subject to new and/or additional development fees and taxes, as adopted by the City Council. (City Attorney)

2. Submit the CC&R's for any area of the project to the Planning Division and the City Attorney's Office for review and approval. (Planning)
3. All geologic studies prepared for the project development shall be made available to prospective property owners by the project sponsor. (Planning)
4. Comply with all applicable sections of the City's Building Security Ordinance, Administrative Rule 7.6. (Police)
5. All glass windows and sliding glass doors shall be equipped with secondary or auxiliary locking devices. (Police)
6. Water service will be provided by the City of Vallejo following completion of the required water system improvements and payment of applicable fees. Performance and payment bonds shall be provided to the City of Vallejo prior to construction of water system improvements. Fees include those fees specified in the Vallejo Municipal Code, and fees for tapping, tie-ins, inspections, disinfection, construction water, and other services provided by the City with respect to the water system improvements. The Water Superintendent may be contacted for description of applicable fees. (Water)
7. Backflow prevention devices are required on water service connections to irrigation systems, certain commercial water users, and to commercial fire sprinkler systems, per City Ordinance 922 N.C. (2d). (Water)
8. Direct roof drainage across non paved areas prior to entering inlets and gutters, when feasible. (VSFCD)
- (M) 9. The project shall comply with the requirements of the Solano County Mosquito Abatement District during all phases of construction and operation. (SAP)
10. Liquefiable material at or near the ground surface should be replaced or recompacted before it is used for structural support. (SAP)
11. The commercial area shall be designed as a single, unified project. The architectural style or "theme" shall create a unique, exciting and quality shopping environment. (SAP)
12. All landscaped open space areas shall be defensively designed to protect the users of these areas and the surrounding land uses. (SAP)
- (M) 13. The project sponsor shall be responsible for the removal and disposal of all siltation that occurs in any off-site retention ponds required for the project. (SAP)
14. The project shall comply with the requirements of the Greater Vallejo Recreation District for those recreation fees and facilities required by the City and GVRD. (SAP)

15. The project sponsor shall participate with the City, Greater Vallejo Recreation District, other agencies, groups, and/or area project sponsors in providing a regional trail system in the Sulphur Springs Mountain area. (SAP)
16. The homeowner association(s) shall participate in a fire prevention program in conjunction with the City's Fire Prevention Division and a crime prevention program in conjunction with the City's Crime Prevention Division. (SAP)
- (M) 17. The project sponsor, City and other concerned agencies and groups shall develop and implement a Hazardous Materials Plan for the project to include the mitigation as described on pages 4-209-4-210 of the DSEIR volume if hazardous materials are used within the Business Park. (SAP)
18. The project sponsor shall pay all applicable development and plan review fees required by Vallejo Sanitation and Flood Control District. (SAP)
19. Additional traffic signals within the project area may be required from the project sponsor at intersections not listed in the SAP. (SAP)
20. All permits from all jurisdictional agencies shall be kept current. (SAP)
21. All open space shall be accessible by emergency vehicles. (SAP)
22. The project sponsor shall reserve a site or space within the office or business areas for a child care center. If, after five years from October 25, 1988, this site or space is still vacant, then this condition is hereby repealed. (SAP)
- (M) 23. Establish with the City and other area project sponsors a Geologic Hazard Abatement District over part or all of the project site if the City requires such a district. (SAP)
- (M) 24. If the City does not implement a citywide Transportation System Management ordinance within two years of October 25, 1988, then the project sponsor shall develop a TSM program within three years of October 25, 1988. Such program should include information regarding the availability of the following:
 - a. Linkages between the project and the San Francisco-Vallejo ferry service;
 - b. Linkages between the project and BART;
 - c. Bus shuttles within the project area;
 - d. Van pools to employment centers outside the project area. (SAP)
25. If required by the City, the project sponsor shall participate in a benefit district to improve that section of Admiral Callaghan Lane

that is currently two lanes north of Redwood Parkway. Such participation shall be limited to the project's impact on this section of right-of-way. (SAP)

- (M) 26. Within three years of October 25, 1988, the park and ride lot shall be designed and constructed by the project sponsor. The design shall accommodate transit and bicycle circulation and service. The maintenance of the lot shall be by the City or other agency or group deemed appropriate using funds generated by assessment and/or Mello-Roos district financing. (SAP)
27. All progress reports and daily reports generated by the project soils engineer, soils technicians and other firm representatives of the soils engineer regarding site grading must be made available to the City in a timely manner and no later than a week after generation of such report to provide the City with current information relative to the grading operation. (SAP)
28. The open space parcels must be free and clean of toxic substances, stock piles of soils, non-approved boulder placements, combustible piles of leaves or branches on the ground and building construction debris as determined by the City and the project soils engineer. Combustible branches and leaves below 12 feet, measured from the bottom of any tree within 100 feet of structures and fences must be removed, and other open space lands subject to the Uniform Fire Code, Division II, "Combustible and Flammable Materials," Section 11.201(a). The existing natural setting of the open spaces must be preserved and maintained except that of approved installations or modifications for landscaping, grading, concrete ditches, fire breaks, trail systems or other approved changes on the contour must preserve the appearance of the natural resource.
- Open space parcels shall not be used as borrow or disposal areas for any material. No grading shall take place on these parcels unless specifically approved by the City. (SAP)
29. Open space parcels owned/controlled by the project sponsor must be free and clear of toxic substances, stock piles of soils, non-approved boulder placements, combustible piles of leaves below 12 feet, measured from the bottom of any tree within 100 feet of structures and fences must be removed, and other open space lands are also subject to the Uniform Fire Code, Division II, "Combustible and Flammable Materials," Section 11.201(a). The existing natural setting of the open spaces must be preserved and maintained except that of approved installations or modifications for landscaping, grading, concrete ditches, fire breaks, trail systems or other approved changes on the contour must preserve the appearance of the natural resource. (SAP)
30. The illustrative plans submitted as part of the June 1988 SAP are for discussion purposes only, and shall not be considered as approved site plans. The specific review of site plans for approval shall occur during the subdivision, unit plan and/or use permit application processes. (SAP)

31. The project is subject to the following Fire Department standards:
 - a. #200 - Planning Division review;
 - b. #205 - Fire Lane requirements;
 - c. #205 - Premises identification;
 - d. #500 - Fire Flow Calculations; and
 - e. Residential Fire Sprinkler Systems. (Fire)
32. The undeveloped open space north of Columbus Parkway, excluding parcel "L" shall be dedicated to the City. (Planning)
33. The project shall comply with the City's Weed Abatement Ordinance. (Fire)
34. The owner shall pay the City charges required by Solano County for providing copies of the project recorded maps to the City. (Current charges are \$7.00 for each sheet plus shipping). (Public Works)
35. Provide to the Public Works Department all computer mapping and graphics information generated for the project in suitable formats usable by the City at the completion of the project. (Public Works)
36. The subdivision/project design engineer shall be responsible for the project plans and must follow through during construction for any changes made from the approved plans subject to City approval. (Public Works)
37. If building construction will take place within the subdivision after the completed subdivision improvements and the dedication of public rights of way for public use are accepted by the City, the public rights of way shall not be used for staging building construction activities, including but not limited to storing construction material and equipment. The street and sidewalks must be kept free of construction debris, mud, and any obstacles and must remain opened to traffic at all times. Streets serving occupied units shall be subjected to the same conditions. (Public Works)
38. The existing Columbus Parkway consists of two 12-foot travel way and 8-foot wide shoulder. The shoulder section is not adequate to handle truck traffic. The design of Columbus Parkway widening must address upgrading the pavement section. Also, depending on condition of the street at the time of construction of the street an overlay may be required. (Public Works)
39. The project sponsor shall comply with the phasing plan by beginning the development of the project with non-residential uses. (Planning)

U.C. BERKELEY LIBRARIES



C124909401

